

Mobility and Beautification

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3.1 Introduction

This chapter provides recommendations and guidelines for the main thoroughfares that provide connectivity within the Town Center Specific Plan area, including Mission Gorge Road, Cuyamaca Street, Town Center Parkway, Riverview Parkway, Park Center Drive, Cottonwood Avenue, and Park Avenue. This section also addresses bicycle facilities, pedestrian facilities, transit services, and on-street parking improvements.

The Specific Plan envisions minimizing conflicts between vehicular traffic and bicycle circulation and provides standards for the establishment of a clearly defined pedestrian and bicycle circulation system that provides connectivity to the natural features within the area and the San Diego River.

This chapter also addresses streetscape beautification within the Specific Plan area. Streetscape beautification encompasses the elements of a street that are part of the public realm and enhance the community's character, including roadways and road intersection elements, such as median and crosswalks, sidewalks, street furniture, street trees and corner treatments.

The Town Center Specific Plan provides recommendations and guidelines for enhancing the area's streetscape and sense of place through the incorporation of attractive street furnishing and treatments, landscape treatments as part of medians, parkways, and landscaped frontages as part of new development.

The Specific Plan envisions the creation of a pedestrian-oriented environment in the area by incorporating human-scale frontages and enhancing facilities that support pedestrian and bicycle movement along the major roadways within the Specific Plan area.

3.2 Major Roadways

This section provides the roadway facility guidelines and standards, across all modes of travel, for all the Mobility Element roadways that traverse the Town Center Specific Plan area. Existing descriptions of each roadway are provided along with proposed changes where applicable. The cross sections shown represent the existing conditions, or where applicable, the proposed alterations. Existing facility standards as well as the proposed changes that will be implemented along the roadways with the build out of the specific plan are outlined in [Tables 3-1 through 3-12](#). It should be noted that the identified cross-sections and standards represent the most constrained sections of the identified roadway segment, thus these should be considered the minimum standards.

3.2.1 Town Center Parkway - Mission Gorge Road to Cuyamaca Street

No changes are proposed along this segment of Town Center Parkway.

Figure 3-1: Town Center Parkway - Mission Gorge Road to Cuyamaca Street

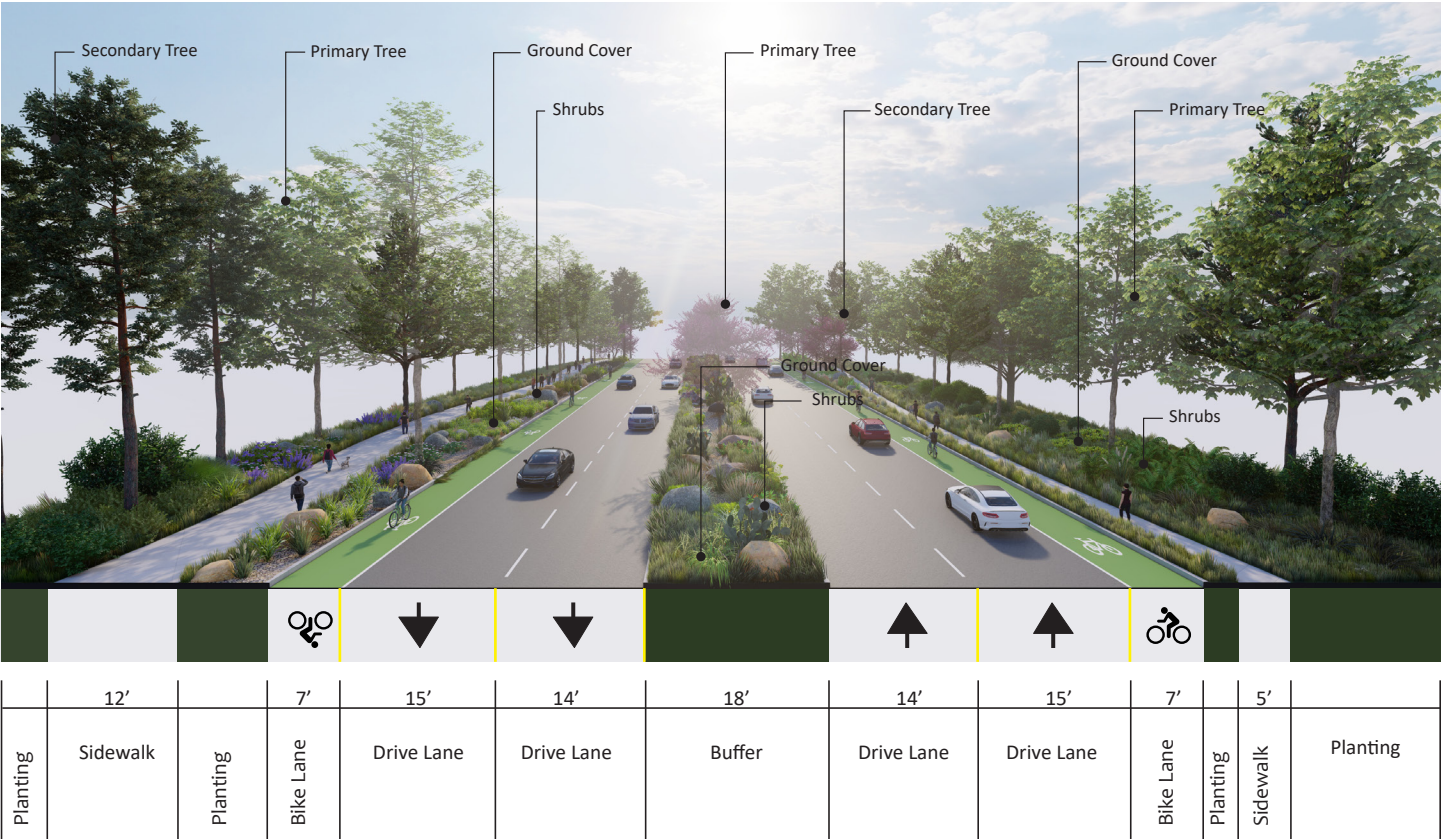


Table 3-1: Town Center Parkway - Mission Gorge Road to Cuyamaca Street

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on both sides of the roadway	No proposed changes
Bike	Class II bike lanes in both directions	No proposed changes
Transit	Route: MTS Bus Route 834 Bus Stops: Town Center Pkwy & 101 Town Center & 140 Town Center & 3205 Town Center Pkwy & Cuyamaca St	No proposed changes
Auto	Travel Lanes: 4 Center: Raised Median Speed Limit: 35 mph	No proposed changes
Parking	Private and public clubs (YMCA, YWCA)	No proposed changes
Right-of-Way	Curb to Curb: 90' Total: 150- 156'	No proposed changes

3.2.2 Town Center Parkway - Cuyamaca Street to Parc One

Figure 3-2: Town Center Parkway - Cuyamaca Street to Parc One

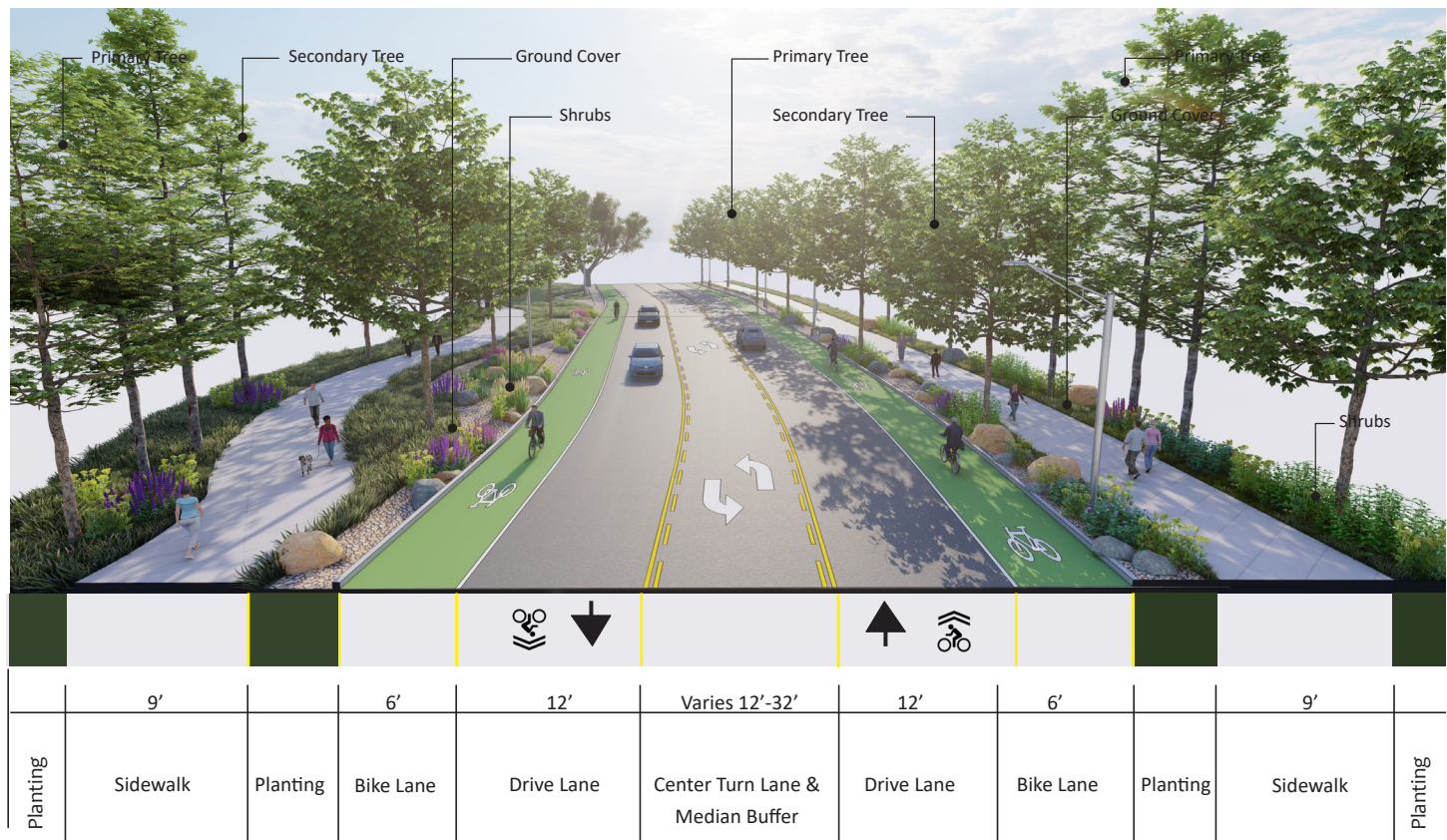


Table 3-2: Town Center Parkway - Cuyamaca Street to Parc One

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on both sides of the roadway	No proposed changes
Bike	Class II bike lanes in both directions	Change to a Class III Bike Route with Sharrows in both directions
Transit	Route: MTS Bus Route 834 Bus Stops: None	No proposed changes
Auto	Travel Lanes: 2 Center: Painted Median Speed Limit: 25 mph	Removed painted median
Parking	None	Implement parallel parking on both sides of the roadway
Right-of-Way	Curb to Curb: 46' Total: 150- 156'	Curb to Curb: 42' Total: 150- 156'

3.2.3 Town Center Parkway - Parc One to Riverview Parkway

Figure 3-3: Town Center Parkway - Parc One to Riverview Parkway

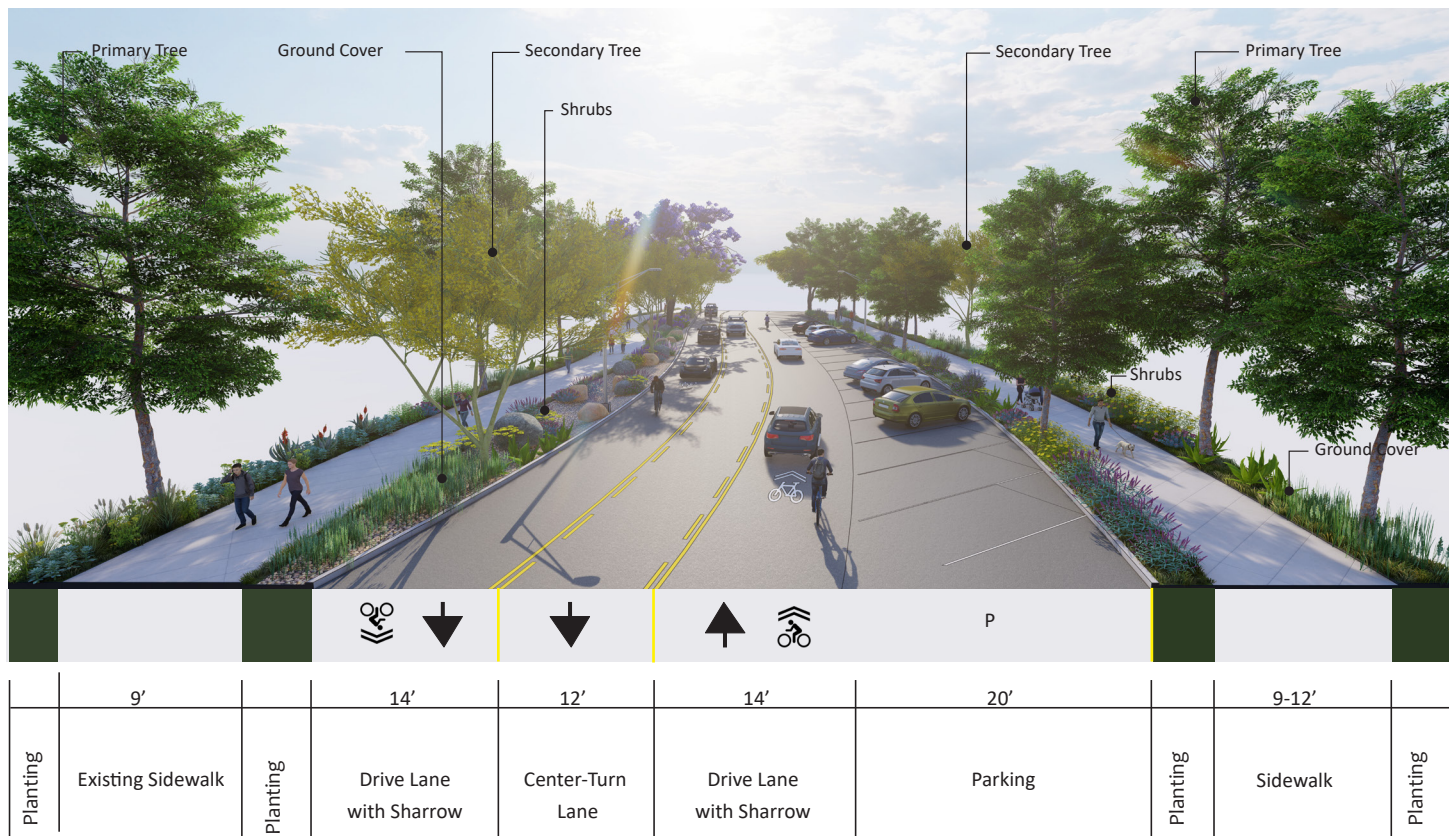


Table 3-3: Town Center Parkway - Parc One to Riverview Parkway

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on both sides of the roadway	No proposed changes
Bike	Class III Bike Routes (Sharrows) in both directions	No proposed changes
Transit	Route: MTS Bus Route 832, 833 & 834 Bus Stops: Santee Town Center	Move Santee Town Center bus Stops to the curb line.
Auto	Travel Lanes: 2 Center: Painted Median / Turn Lane Speed Limit: 25 mph	Narrow travel lanes from 14' to 12' Narrow median / turn-lane from 12' to 10'
Parking	None	Implement angled parking on the northeast side of the roadway
Right-of-Way	Curb to Curb: 40' Total: 90'	Curb to Curb: 54' Total: 90'

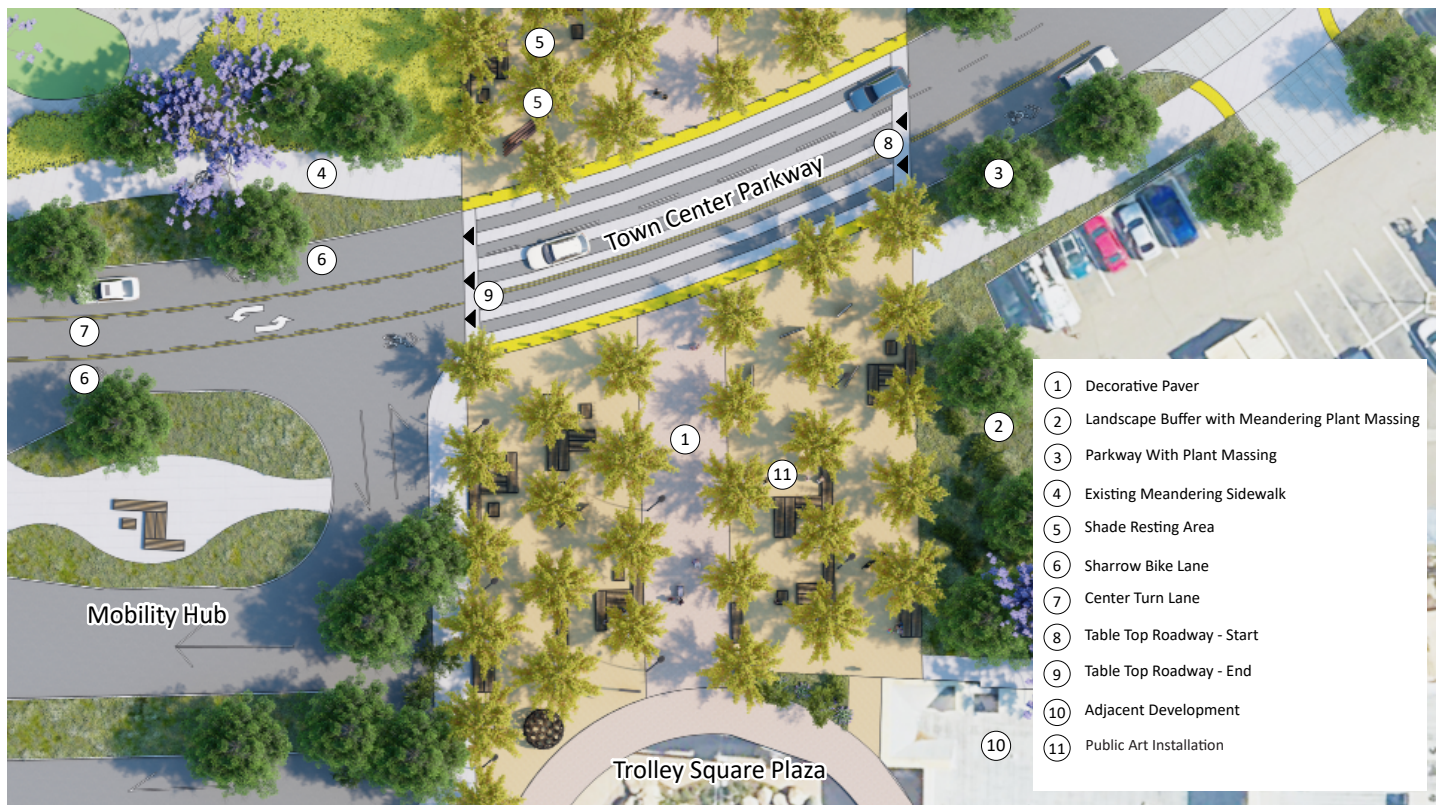
Other Proposed Improvements

Within the Arts and Entertainment Neighborhood, a safe, inviting, and convenient connection across Town Center Parkway from Trolley Square to the Town Center Core and Bus Station will help the user experience and will be a valuable feature in ensuring the success of the neighborhood. Therefore, the following improvements are recommended:

- ▶ Create pedestrian pathway from the Trolley Station to the Town Center Core and Bus Station.
 - ▷ This would include decoupling the driveways for the MTS Bus Station and Parking lot to the southeast on Town Center Parkway to remove modal conflicts and allow the pathway clear access to the area.
 - ▷ Separate driveways for each site would be provided on either side of the pathway, as shown in the graphic below.
 - ▷ Potentially install a raised crosswalk where the pathway crosses Town Center Parkway.
 - ▷ Potentially install a pedestrian activated signal where the pathway crosses Town Center Parkway.
- ▶ Coordinate with MTS to explore reducing the bus parking/transfer area and/or moving the bus stops to on-street parking to allow for better viability and access.
- ▶ Coordinate with MTS to explore the opportunity of building a parking structure over the existing Bus Station that would serve both the Arts and Entertainment Neighborhood and MTS riders.

Some of these improvements are displayed, conceptually, in the figure below.

Figure 3-4: Additional Improvements at Town Center Parkway



3.2.4 Cuyamaca Street - Mission Gorge Road to Town Center Parkway

No changes are proposed along this segment of Cuyamaca Street.

Figure 3-5: Cuyamaca Street - Mission Gorge Road to Town Center Parkway

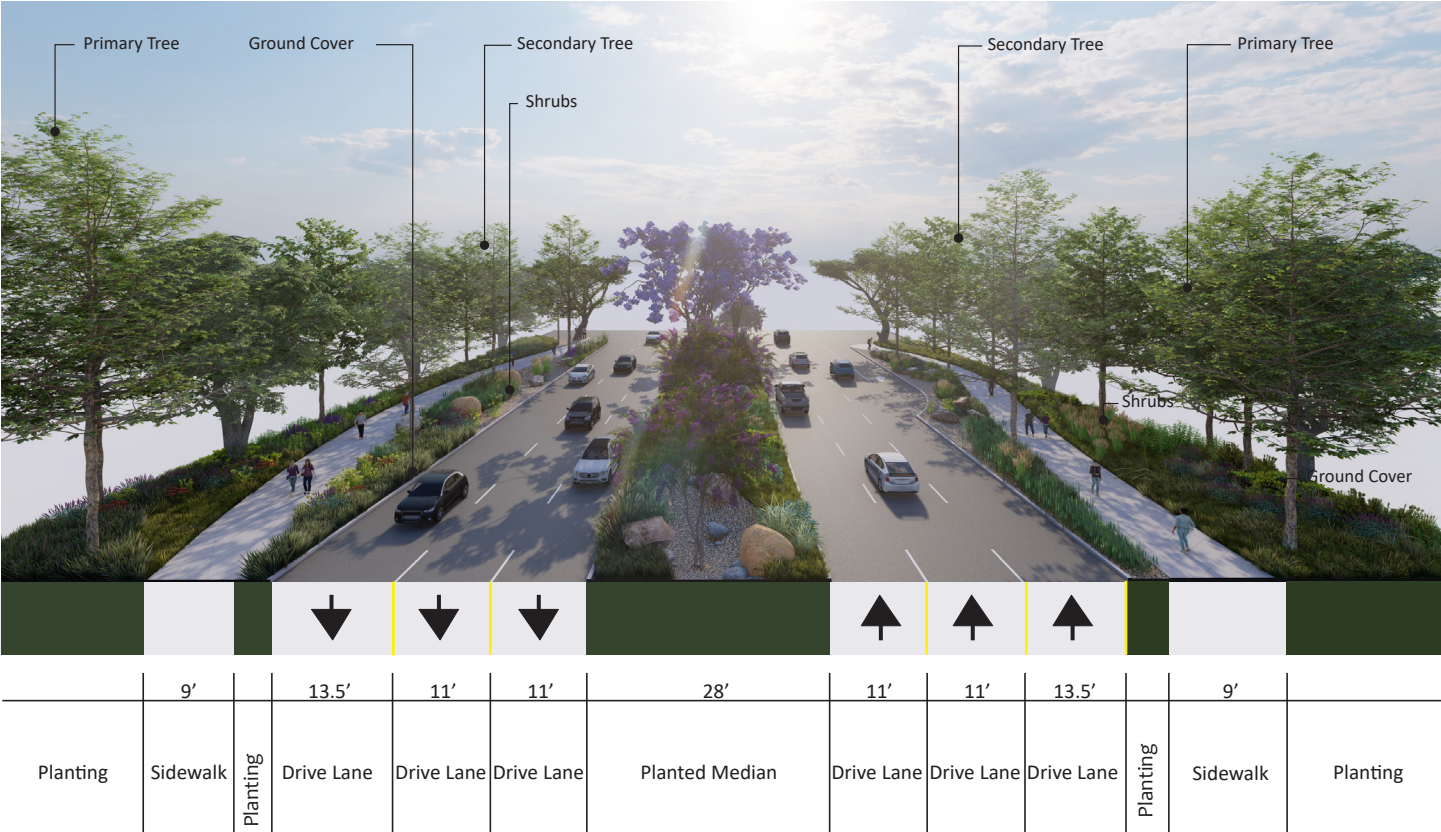


Table 3-4: Cuyamaca Street - Mission Gorge Road to Town Center Parkway

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on both sides of the roadway	Implement missing segments of the multi-use pathway on the west side of the roadway
Bike	None	No proposed changes
Transit	None	No proposed changes
Auto	Travel Lanes: 6 Center: Raised Median Speed Limit: 35 mph	No proposed changes
Parking	None	No proposed changes
Right-of-Way	Curb to Curb: 98' Total: 158'	No proposed changes

3.2.5 Cuyamaca Street - San Diego River Bridge to Town Center Parkway

Figure 3-6: Cuyamaca Street - San Diego River Bridge to Town Center Parkway

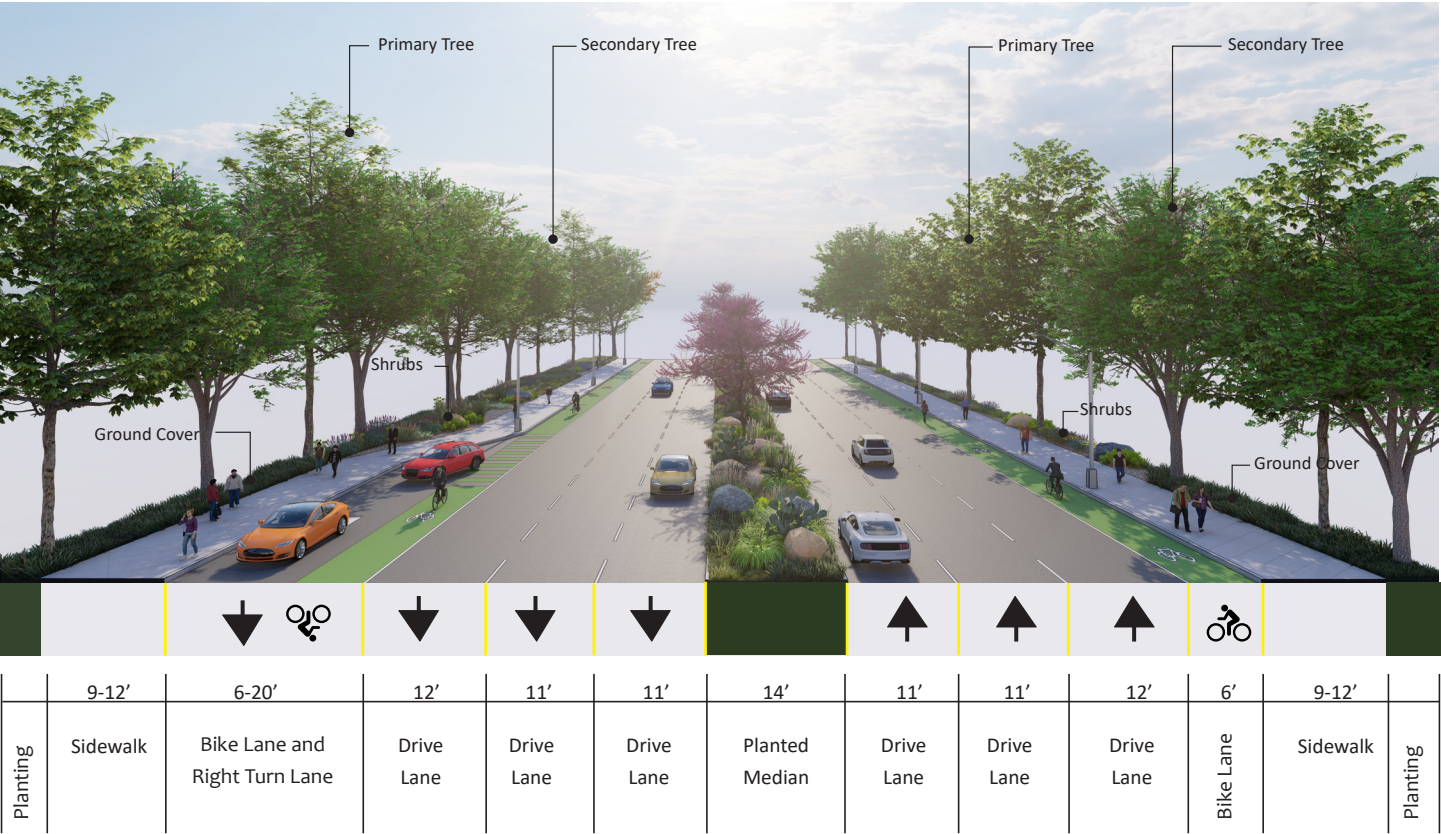


Table 3-5: Cuyamaca Street - San Diego River Bridge to Town Center Parkway

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on the west side of the roadway	Implement a multi-use pathway on the east side of the roadway
Bike	Class II bike lanes in both directions	No proposed changes
Transit	Route: MTS Bus Route 832 Bus Stops: Cuyamaca St & Riverview Dr	No proposed changes
Auto	Travel Lanes: 4 Center: Raised Median / Painted Median Speed Limit: 35 mph	No proposed changes
Parking	None	No proposed changes
Right-of-Way	Curb to Curb: 58' Total: 78'	Curb to Curb: 58' Total: 93'

3.2.6 Riverview Parkway - Mission Gorge Road to Cottonwood Avenue

Figure 3-7: Riverview Parkway - Mission Gorge Road to Cottonwood Avenue

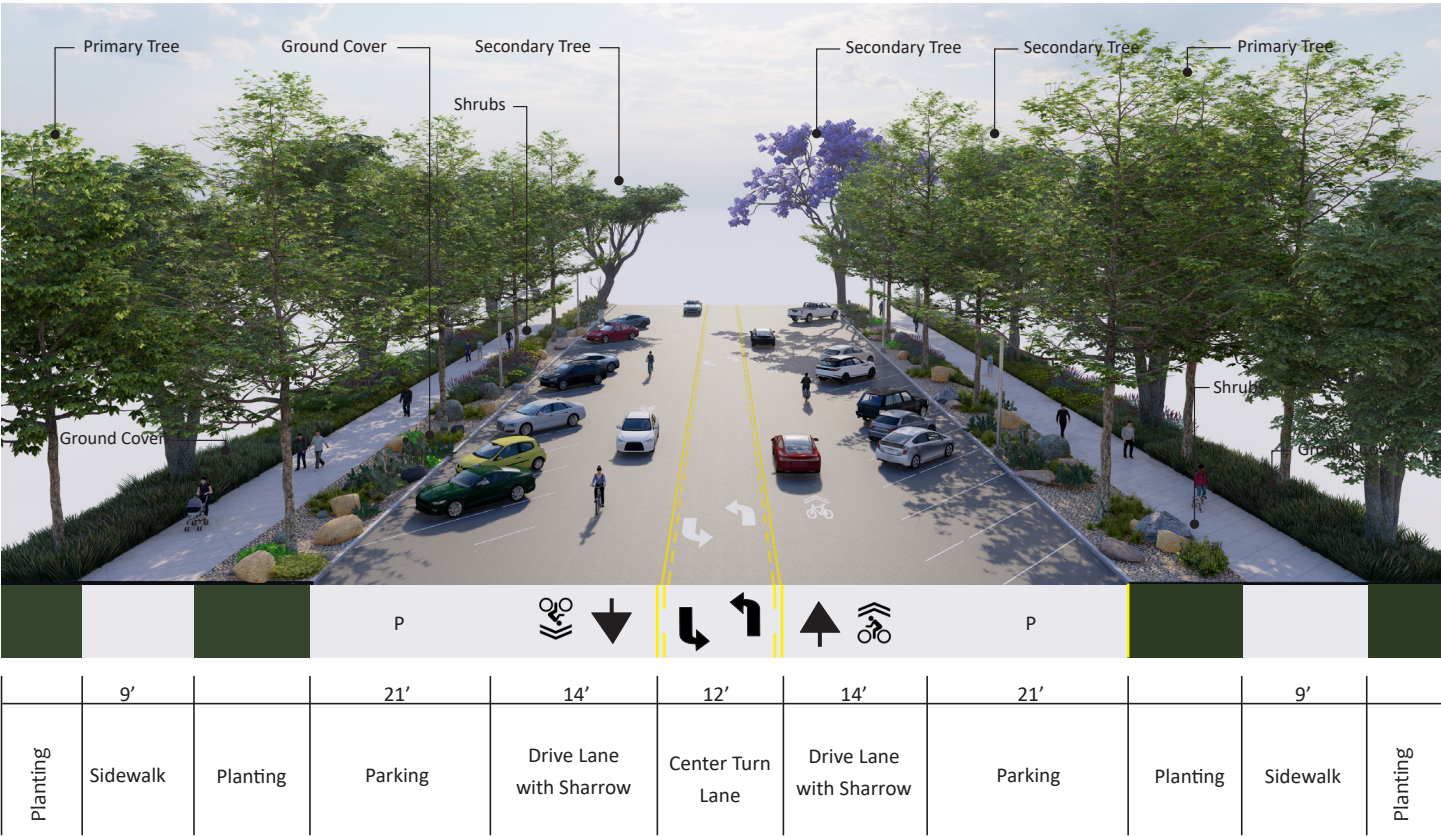


Table 3-6: Riverview Parkway - Mission Gorge Road to Cottonwood Avenue

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on both sides of the roadway	No proposed changes
Bike	Class II bike lanes in both directions	Change to a Class III Bike Route (Sharrows) in both directions
Transit	Route: MTS Bus Route 832 & 833 Bus Stops: None	No proposed changes
Auto	Travel Lanes: 4 Center: Raised Median / Painted Median Speed Limit: 30 mph	Change median from 18’ striped median to a 12’ continuous left-turn lane Reduce number of travel lanes from 4 to 2
Parking	None	Implement angled parking on both sides of the roadway
Right-of-Way	Curb to Curb: 82’ Total: 152’	No proposed changes

3.2.8 Riverview Parkway - Cottonwood Avenue to Magnolia Avenue

The additional right-of-way required to construct this roadway to its ultimate classification has already been allocated.

Figure 3-8: Riverview Parkway - Cottonwood Avenue to Magnolia Avenue

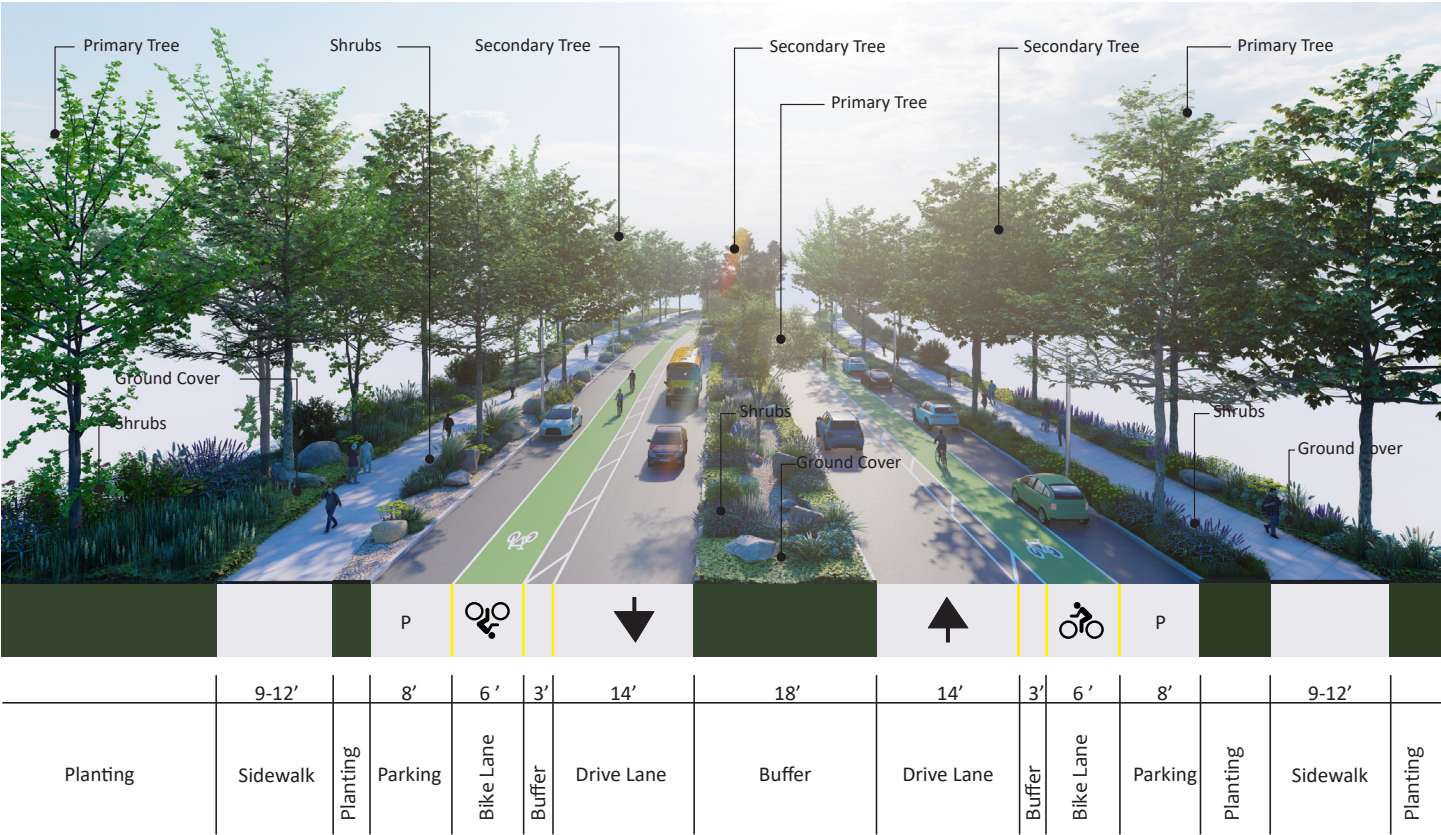


Table 3-7: Riverview Parkway - Cottonwood to Magnolia Avenue

Mode	Existing	Proposed Changes
Pedestrian	Multi-use pathways on south side of the roadway	Implement a multi-use pathway on the north side of the roadway (River Trail)
Bike	None	Implement a Class II bike lanes in both directions
Transit	None	No proposed changes
Auto	Travel Lanes: 2 Center: None Speed Limit: 35 mph	Implement a 18' median
Parking	None	Implement parallel parking on both sides
Right-of-Way	Curb to Curb: 32' Total: 67'	Curb to Curb: 82' Total: 150'

3.2.9 Riverwalk Drive - Cuyamaca Street to Park Center Drive

No changes are proposed to this segment of Riverview Drive.

Figure 3-9: Riverwalk Drive - Cuyamaca Drive to Park Center Drive

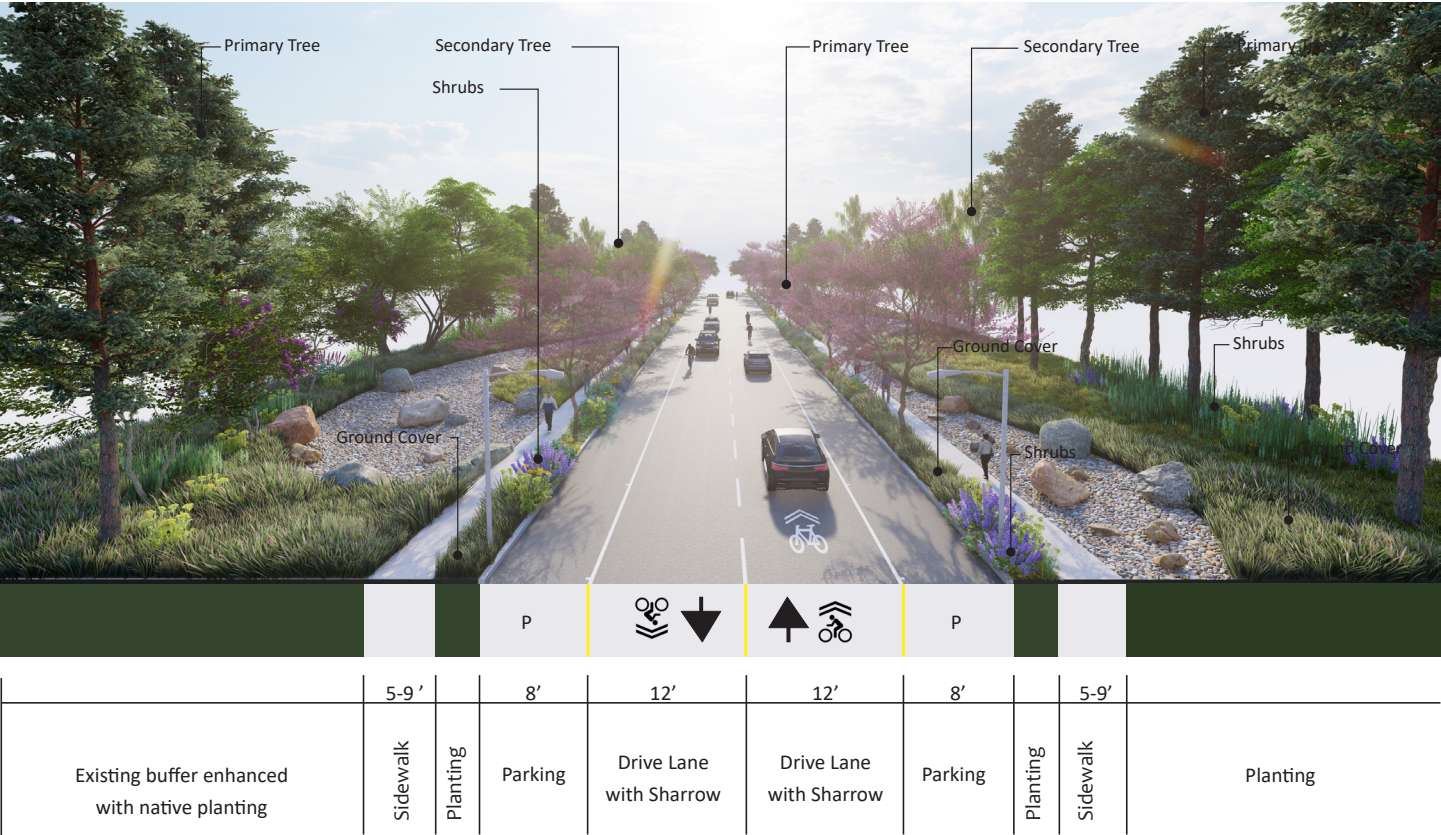


Table 3-8: Riverwalk Drive - Cuyamaca Street to Park Center Drive

Mode	Existing	Proposed Changes
Pedestrian	5' Sidewalks on both sides with 4' planted parkways	No proposed changes
Bike	Class III Route (Sharrows) in both directions	No proposed changes
Transit	None	No proposed changes
Auto	Travel Lanes: 2 Center: None Speed Limit: 25 mph	No proposed changes
Parking	Parallel Parking on both sides	No proposed changes
Right-of-Way	Curb to Curb: 40' Total: 64'	No proposed changes

3.2.10 Cottonwood Avenue - Park Center Drive to Riverview Parkway

This is a future segment of Cottonwood Avenue that will provide a new north/south connection across the San Diego River.

Figure 3-10: Cottonwood Avenue - Park Center Drive to Riverview Parkway

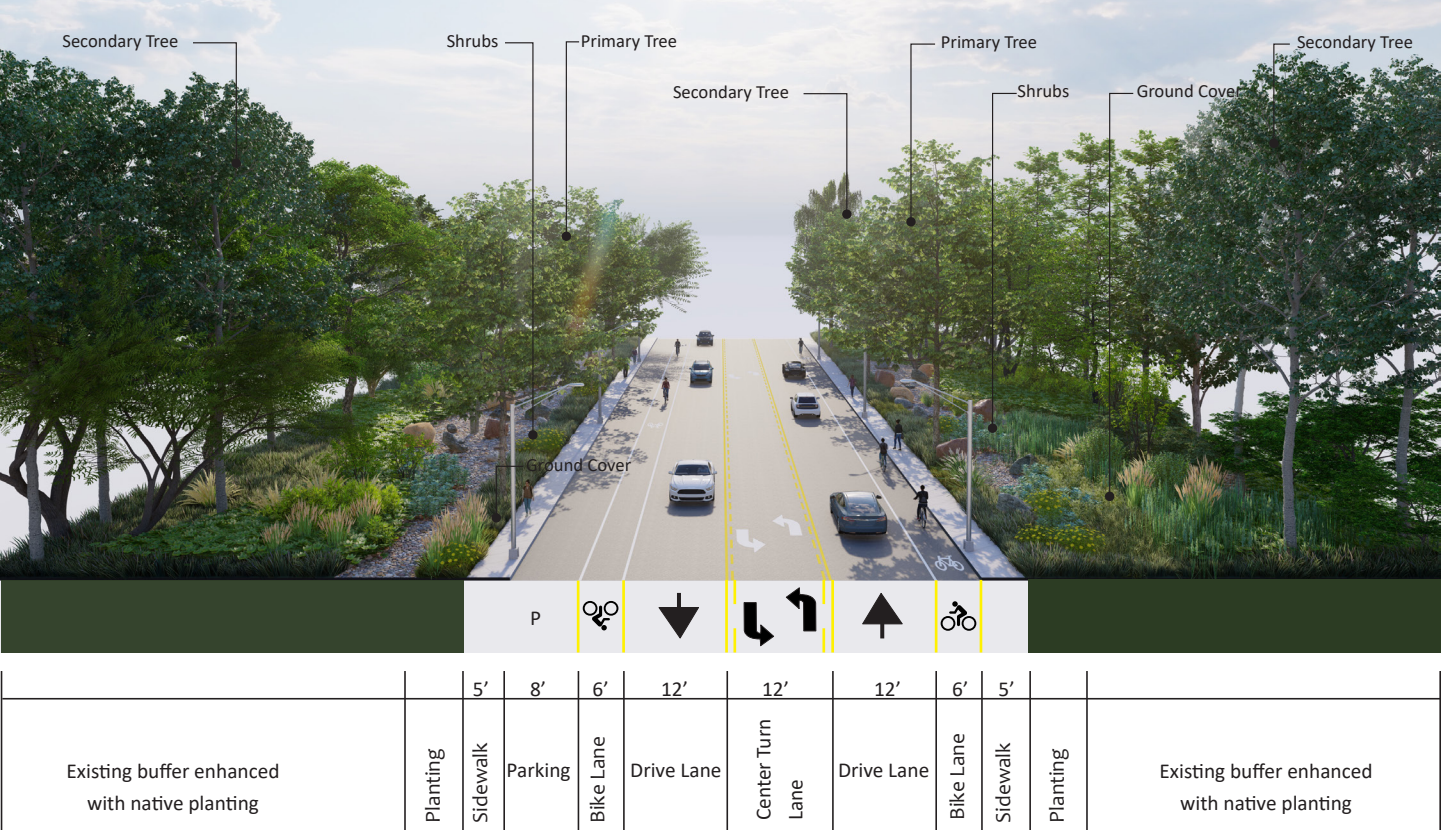


Table 3-9: Cottonwood Avenue - Park Center Drive to Riverview Parkway

Mode	Existing	Proposed Changes
Pedestrian	Does not currently exist	5' Sidewalks on both sides
Bike		Class II Bike Lanes in both directions
Transit		None
Auto		Travel lanes: 2 Center: Continuous Left-Turn Lane
Parking		Parallel parking on the west side of road, north of the San Diego River bridge.
Right-of-Way		Curb to Curb: 54' Total: 64'

3.2.11 Main Street - Riverview Parkway to Cottonwood Avenue

This is a new roadway that will create an east/west connection between the Arts and Entertainment Neighborhood and the Park Avenue Residential Neighborhood. This segment should be designed as a Main Street concept.

Figure 3-11: Main Street - Riverview Parkway to Cottonwood Avenue



Table 3-10: Main Street - Riverview Parkway to Cottonwood Avenue

Mode	Existing	Proposed Changes
Pedestrian	Does not currently exist	8' Sidewalks on both sides with 4' planted parkways
Bike		Class III Bike Routes (Sharrows) in both directions
Transit		None
Auto		Travel lanes: 2 Center: Undivided
Parking		None
Right-of-Way		Curb to Curb: 40' Total: 64'

3.2.12 Typical Residential Roadway

This is the typical standard that internal, non-classified, residential roadways, within the specific plan area should be designed to.

Figure 3-12: Typical Residential Roadway

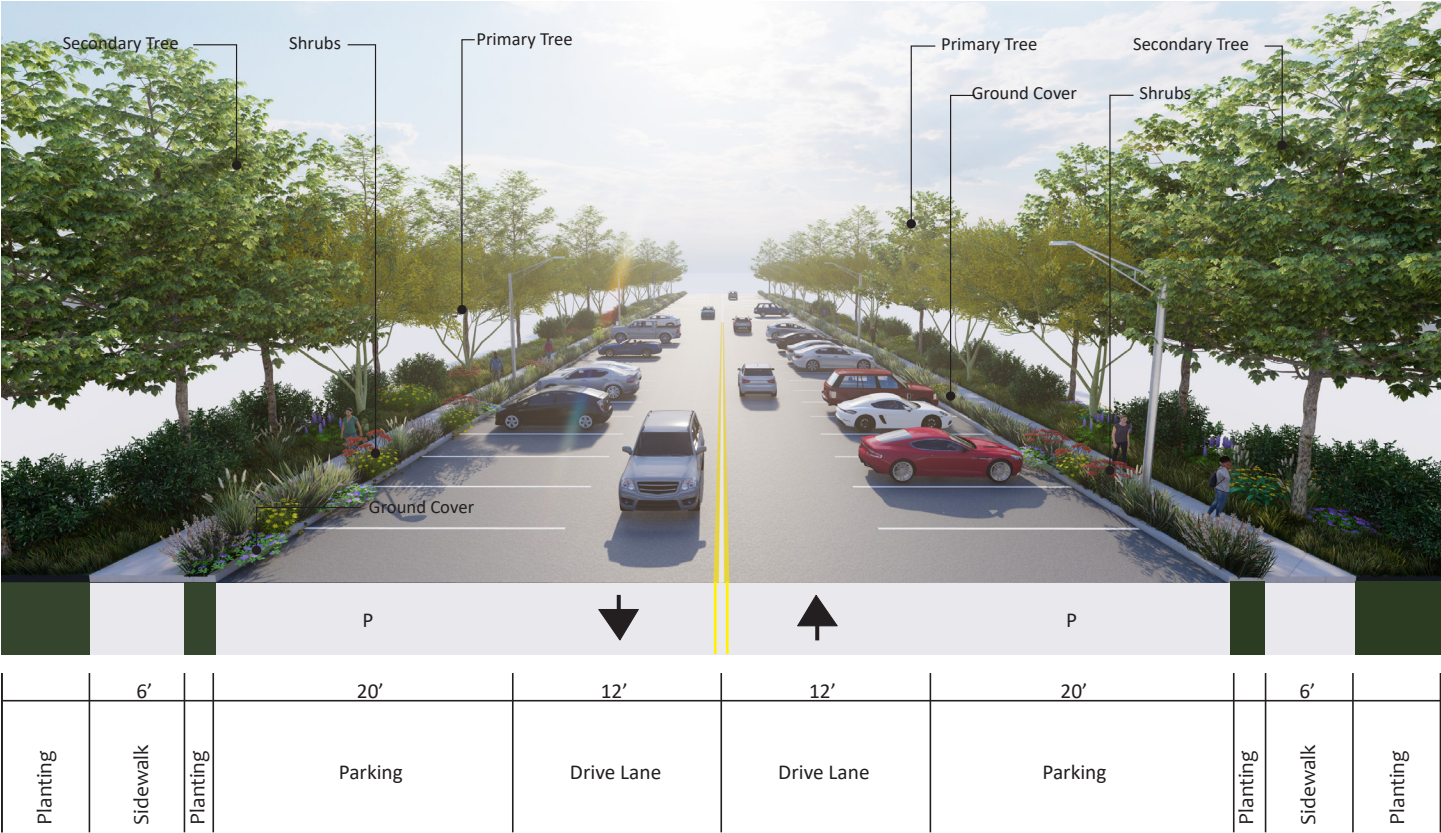


Table 3-11: Typical Residential Roadway

Mode	Facility
Pedestrian	6' Sidewalks on both sides with 2' planted parkways
Bike	None
Transit	None
Auto	Travel lanes: 2 Center: None Speed Limit: 25 mph
Parking	Perpendicular parking on both sides of the roadway
Right-of-Way	Curb to Curb: 64' Total: 80'

3.2.13 Park Center Drive - From Mast Boulevard to Magnolia Avenue

This is a future segment of Park Center Drive that will create a north/south and east/west connection between Mast Boulevard and Magnolia Avenue.

Figure 3-13: Park Center Drive - Mast Boulevard to Magnolia Avenue

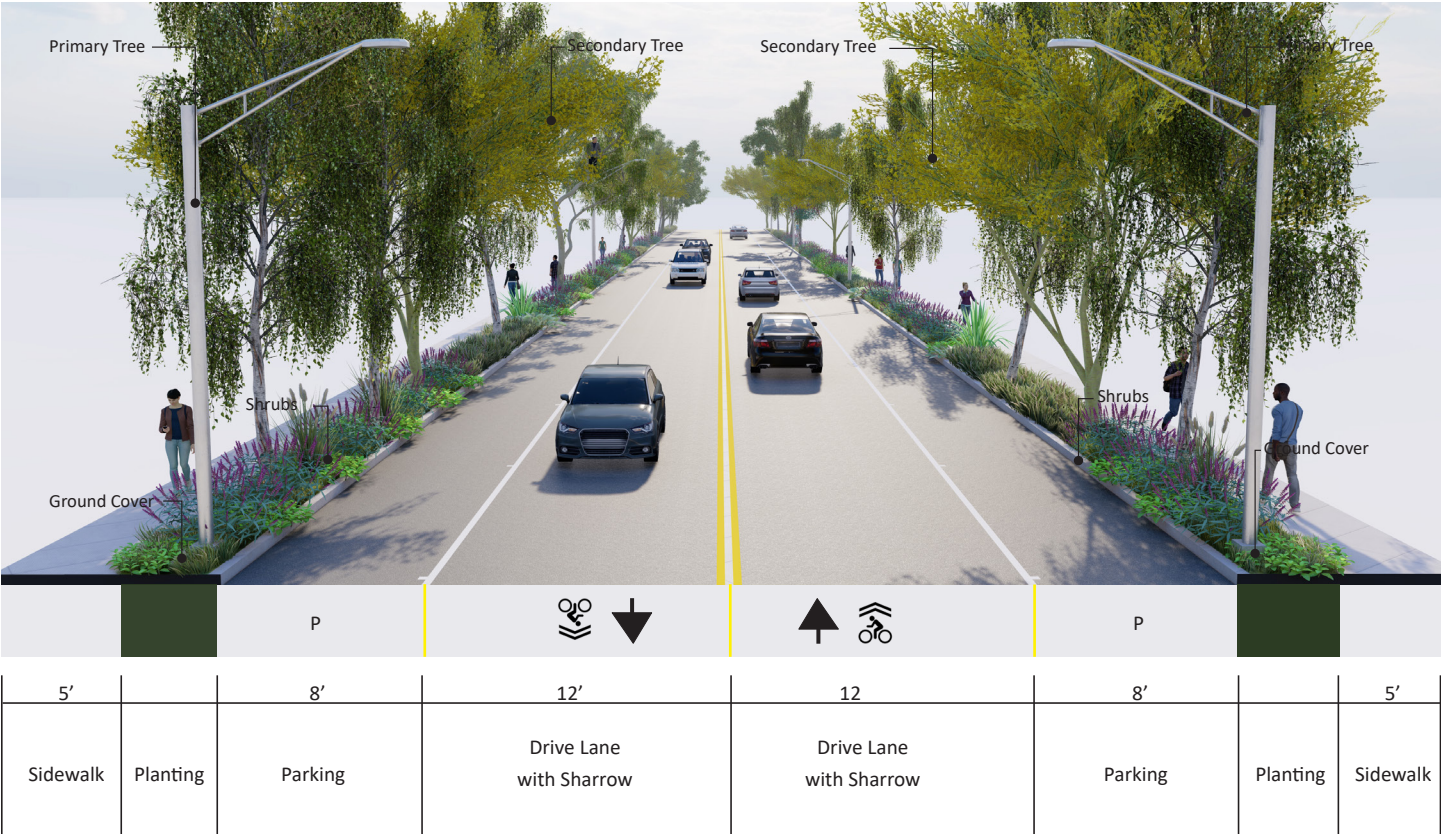


Table 3-12: Park Center Drive - Mast Boulevard to Magnolia Avenue

Mode	Existing	Proposed Changes
Pedestrian	Does not currently exist	5' Sidewalks on both sides with 4' planted parkways
Bike		None
Transit		None
Auto		Travel Lanes: 2 Center: Undivided
Parking		Parallel Parking on both sides
Right-of-Way		Curb to Curb: 40' Total: 58'

3.3 Proposed Transportation Network

The following section provides an overview of the proposed transportation network that will be implemented in conjunction with the build out of the proposed land uses within the Specific Plan area. Proposed transportation facility maps are provided for each mode of travel, and the specific improvements / changes required to build out the network are also described. These improvements are consistent with the roadway standards that are described in [Section 3.2: Major Roadways](#); however, this section also shows how the separate facilities will connect and work together to provide safe and comfortable connections for all modes of travel.

Multi-Use Pathways

Multi-use pathways will be provided along all major roadway facilities within the specific plan area. Multi-use pathways provide safe, convenient, and comfortable pedestrian access between the different land uses and neighborhoods, and form the backbone of first mile and last mile connections between the transit center and proposed residential uses. Less experienced or slower cyclists can also utilize the multi-use pathways as an alternative to using the in-road facilities such as Class II Bike Lanes and Class III Bike Routes. [Figure 3-14](#) displays the multi-use pathways that currently exist as well as those that need to be implemented as the specific plan area continues to develop.

Pedestrian Connections

Sidewalk facilities are generally provided along all roadways in the Specific Plan area, and provide connectivity to the system of multi-use pathways. [Table 3-13: Missing Sidewalk Facilities](#) identifies the locations where sidewalk facilities are currently not present along roadways within the Specific Plan area. Sidewalk facilities will be implemented along these roadways, where feasible, in conjunction with the adjacent land uses being developed or redeveloped.

Table 3-13: Missing Sidewalk Facilities

<u>Roadway</u>	<u>Segment</u>
Cuyamaca St (east side)	Town Center Pkwy to River Trail bridge (overpass)
Riverview Pkwy (east side)	Town Center Pkwy to North end
Riverview Pkwy (SE side)	Town Center Pkwy to ~400ft south of Town Center Pkwy
Magnolia Ave (west side)	Cottonwood Ave to Park Ave
Riverview Pkwy (north side)	Magnolia Ave to West end (culdesac)
Park Center Dr (east side)	Riverwalk Dr to South end (culdesac)
Cottonwood Ave (west side)	Annie Ln to Claudia Ave
Park Ave (both sides)	Cottonwood Ave to 1st St
3rd St (both sides)	Cottonwood Ave to 1st St
Cottonwood Ave (both sides)	Mission Gorge Rd to North end
Edgemoor Dr (both sides)	Mission Gorge Rd to North end
1st St (west side)	Mission Gorge Rd to Park Ave

Figure 3-14: Multi-Use Pathways



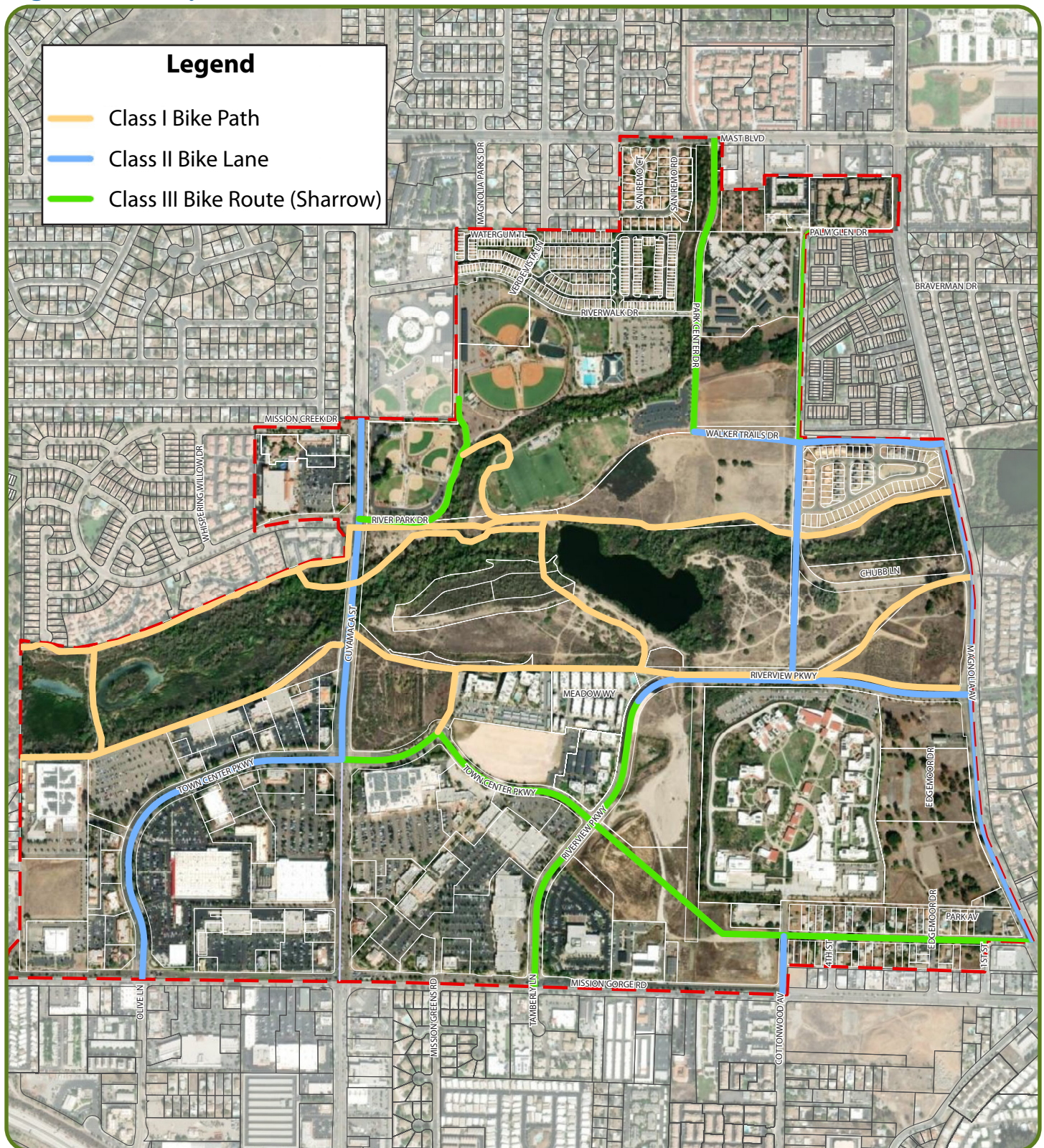
Bicycle Network

As outlined in [Section 3.2: Major Roadways](#), there are several bicycle facility changes or improvements that will be implemented with the buildout of the Specific Plan. [Figure 3-15: Bicycle Network](#) displays the proposed bicycle network within the Specific Plan area, while [Table 3-14](#) (below) identifies the specific changes to the existing bicycle facilities.

Table 3-14: Changes to the Bicycle Network

<u>Segment</u>	<u>Extent</u>	<u>Existing Facility</u>	<u>Proposed Facility</u>
River Trail Crossing	North side of Walmart to River Rock Ct	None	Class I Path
River Trail (south) Segment 10	Existing Class I north of Town Center Roundabout to Planned Town Center Pkwy Class I	None	Class I Path
River Trail (north) Segment 3	Cuyamaca St to Cottonwood Ave	None	Class I Path
Park Center Dr	Riverwalk Dr to Park Center Dr	None	Class III Bike Route (Sharrows)
Cottonwood Ave	Palm Glen Dr to Park Center Dr	None	Class III Bike Route (Sharrows)
Cottonwood Ave (extension)	Park Center Drive to Riverview Parkway	Roadway does not exist	Class II Bike Lanes
Riverview Parkway	Mission Gorge Road to Town Center Parkway	Class II Bike Lanes	Class III Bike Route (Sharrows)
Riverview Parkway	Town Center Parkway to Cottonwood Avenue (extension)	None	Class III Bike Route (Sharrows)
Riverview Parkway	Cottonwood Avenue (extension) to Magnolia Avenue	None	Class II Bike Lanes
Town Center Parkway	Cuyamaca Street to Parc One Driveway	Class II Bike Lanes	Class III Bike Route
Main Street	Riverview Parkway to Cottonwood Avenue	Roadway does not exist	Class III Bike Route (Sharrows)

Figure 3-15: Bicycle Network



Other Proposed Improvements

The Active Santee Plan, January 2021 identifies the need for a crosswalk with pedestrian/bicycle-activated Hybrid Beacon, across Cuyamaca Street at the San Diego River Trail Crossing. This improvement should be implemented in conjunction with the construction of the multi-use pathway on the east side of Cuyamaca Street.

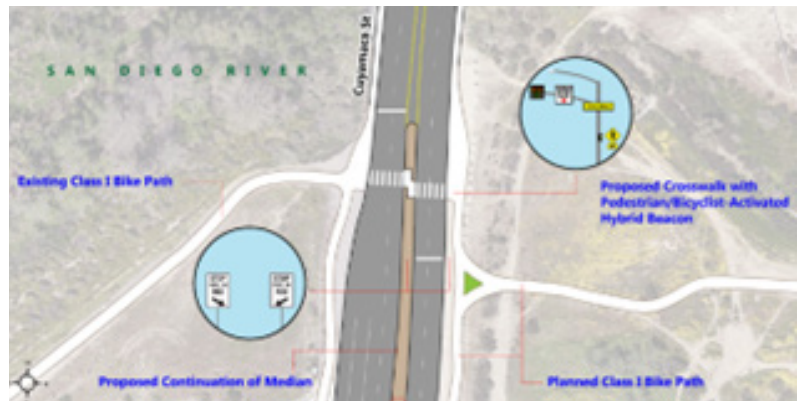


Figure 3-17: Pedestrian/bicycle-activated crosswalk across Cuyamaca Street.

Figure 3-16: Traffic Circle Riverview Parkway and Town Center Parkway



- ① Parkway Planting
- ② New Sidewalk
- ③ Bioswale Parkway
- ④ Center Turn Lane
- ⑤ Sharrow Bike Lane
- ⑥ Angled Parking
- ⑦ Parallel Parking
- ⑧ Storm Water Treatment Planter
- ⑨ Roundabout With Planting, Boulders And Signage

Where Town Center Parkway continues past Riverview Parkway and becomes a new road section called Main Street, a traffic circle is proposed to handle the change in varying right-of-way dimensions at this intersection

Figure 3-18: Roundabout at Town Center Parkway and Parc One



- ① Meandering Planting
- ② Meandering Sidewalk
- ③ Protected Crosswalk
- ④ Pickup/drop-off
- ⑤ Sharrows Bike Lane
- ⑥ Two-way Roadway
- ⑦ Roundabout With Boulders And Signage
- ⑧ Center Turn Lane

Transit Network

Access to quality regional transit services is a defining feature of the Specific Plan area. However, all transit routes and services within the Specific Plan area are operated by the Metropolitan Transit Service (MTS), and are outside of the control of the City of Santee. As such, the transit services and facilities within the Specific Plan cannot be changed without the approval of MTS, thus, no changes to the existing transit network are proposed. **Figure 3-19: Transit Network** displays the existing transit network that serves the Specific Plan area.

Roadway Network

The specific roadway guidelines, standards, and features for all classified roadways, within the specific plan area, are described in detail within **Section 3.2: Major Roadways**. **Figure 3-20: Roadway Network** displays the proposed roadway classifications and identifies the general locations in which the proposed roadway extensions will be located

3.4 Transportation and Parking Goals and Policies

The following are the transportation and parking related goal and policies for the Specific Plan area.

Goal

Provide for the development of a safe, efficient, and cost effective variety of transportation systems to adequately support the mobility needs of the Town Center area with minimal negative impact on the community.

Policies

- ▶ Parking areas, including parking structures, should be strategically located to serve all areas of Town Center and should not act as prominent visual features (see **Figure 3-22: Parking** for potential locations).
- ▶ Adequate off-street parking should be provided throughout the Specific Plan area, wherever possible.
- ▶ A regional Transit Center should be provided to handle local and regional bus and trolley service.
- ▶ Parking facilities should be provided in close proximity to the Transit Center to encourage regional use of the mass transit services provided.
- ▶ Parking structures should be provided to ensure adequate parking while allowing open landscape areas throughout the Town Center Specific Plan area.
- ▶ Extensive landscape, mounding and grade separations for the development of interior and periphery streets are techniques that should be used to de-emphasize the presence of the automobile.
- ▶ Whenever new development comes in, it must build out sidewalks along all roads (public or private) and connect to the existing sidewalk network.
- ▶ New development that incorporates public on-street parking as part of their projects will be able to take credit towards on-site parking requirements.
- ▶ Access points should be well defined, located on major streets, and utilize adequate traffic control features.
- ▶ Alternate vehicular access should be provided to Carlton Hills Boulevard and Mast Boulevard to provide relief to Cuyamaca Street, Magnolia Avenue and Mission Gorge Road.

Figure 3-19: Transit Network



Figure 3-20: Roadway Network

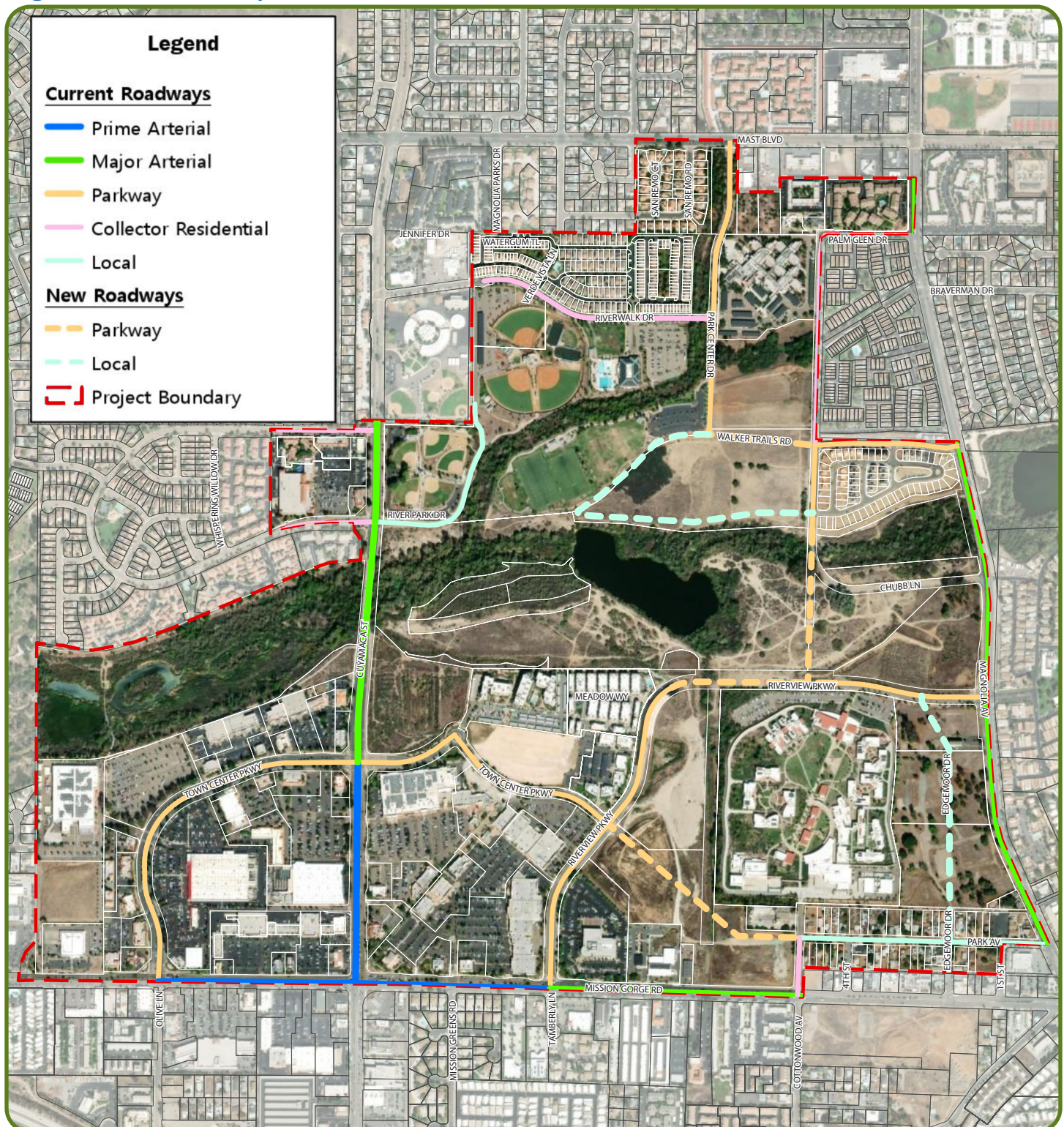


Figure 3-21: Parking



3.5 Streetscape Beautification

The Streetscape Beautification section provides an overarching set of principles for the foundation of public and private improvements in the Town Center. It demonstrates the interconnected network and hierarchy of street corridors, public open spaces, building forms, highlighting the location and character of public and private improvements in the Specific Plan area.

Santee's connection to the surrounding landscape is an undeniable asset, from its mountain trails to the San Diego River habitat which serves as a unique amenity to the Town Center. The intent of the following standards, goals and policies is embed the character of the San Diego River and to allow the landscape to become a unifying element throughout the Specific Plan area between the various neighborhoods that integrates itself as a prominent thread in the fabric of the space. By bringing the river's character into these street improvements it ensures that users are reminded of their unique location within Santee and the amenities that it has to offer.

3.5.1 Street Trees

The character and neighborhood distinction in the of the Specific Plan shall be further enhanced through the use of specific street trees along the Specific Plan roadways, which serve to stitch together the various neighborhoods. [Table 3-15: Street Tree Planting List](#) should be used in coordination with [Figure 3-22: Street Tree Planting](#) to understand where specific species of trees are to be planted.

- ▶ Main Street Tree – Evergreen with large canopy
- ▶ Town Center Tree – Flowering, tall, stately
- ▶ Multimodal Street Tree – Tall, drought-tolerant
- ▶ Native Corridor Tree – Riparian, local
- ▶ Neighborhood Tree – Evergreen
- ▶ Screening Tree – Tall, stately, large canopy

Figure 3-22: Street Tree Planting

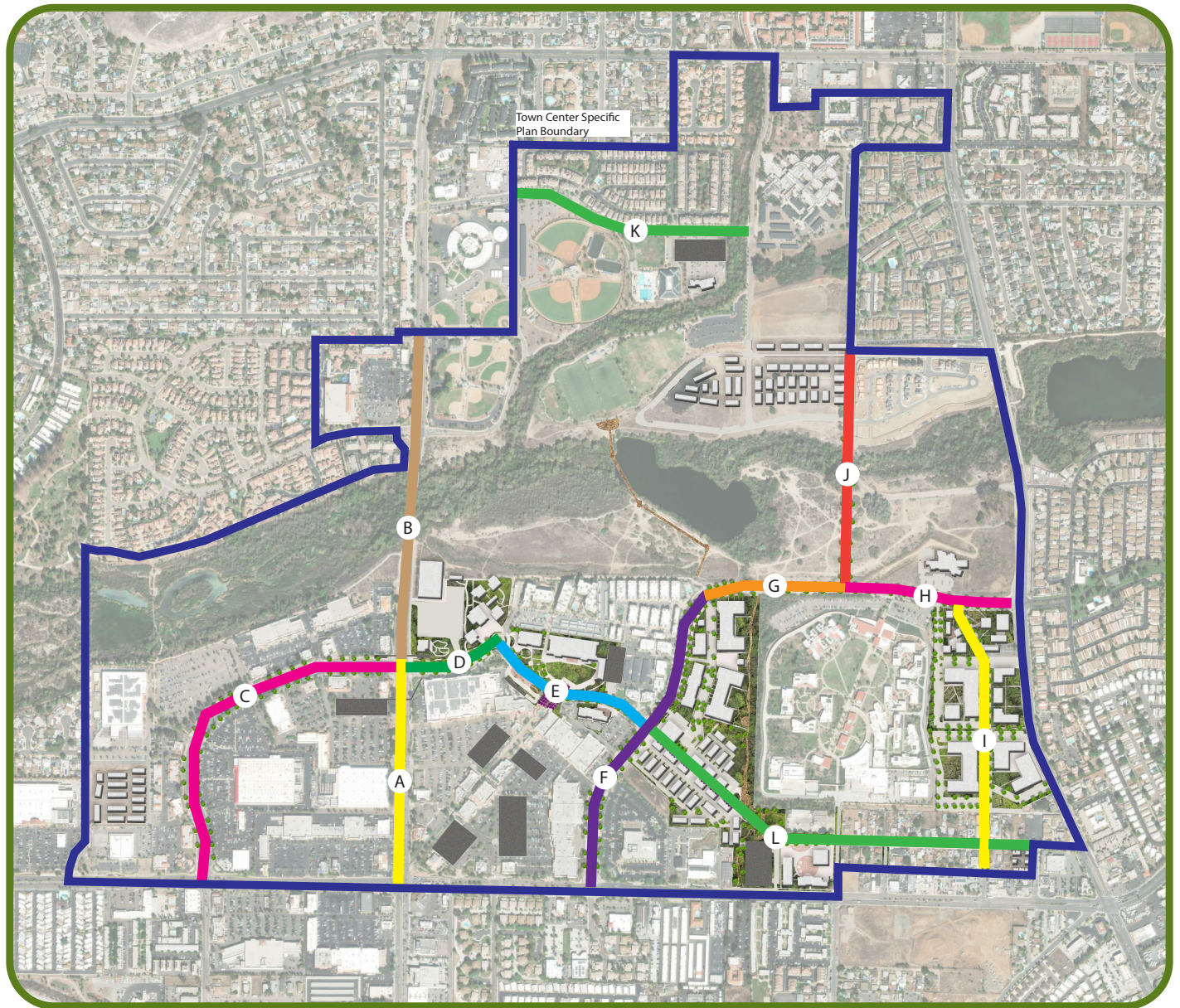


Table 3-15: Street Tree Planting List

Key	Street	Segment	Primary Tree	Secondary Tree
A	Cuyamaca Street	Town Center Parkway to Mission Gorge Road		
		Parkway (up to 9')	Ulmus parvifolia - Chinese Evergreen Elm	Quercus suber - Cork Oak
		Median (up to 28')	Jacaranda mimosifolia - Jacaranda	Koelreuteria bipinnata - Chinese Flame Tree
B	Cuyamaca Street	River Park Drive to Town Center Parkway		
		Parkway (up to 3')	Cercis occidentalis - Western Redbud	Callistemon citrinus - Lemon Scented Bottle Brush
		Parkway (up to 4')	Chitalpa tashkentensis - Chitalpa	Hymenosporum flavum - Sweetshade
C	Town Center Parkway	Mission Gorge Road to Cuyamaca Street		
		Median (up to 18')	Pinus canariensis - Canary Island Pine	Pinus pinea - Italian Stone Pine
		Parkway (up to 3')	Cercis occidentalis - Western Redbud	Chitalpa tashkentensis - Chitalpa
D	Town Center Parkway	Cuyamaca Street to Parc One		
		Parkway (up to 6')	Geijera parviflora - Australian Willow	Lagerstroemia indica - Crape Myrtle
E	Town Center Parkway	Parc One to Riverview Parkway		
		Parkway (up to 4')	Lophostemen confertus - Brisbane Box	Brachychiton populneus - Bottle Tree
		Parkway (up to 9')	Magnolia grandiflora - Southern Magnolia	Bauhinia blakeana - Hong Kong Orchid Tree
F	Riverview Parkway	Mission Gorge Road to Meadow Way		
		Parkway (up to 11')	Quercus suber - Cork Oak	Koelreuteria bipinnata - Chinese Flame Tree
		Parkway (up to 12')	Quercus ilex - Holly Oak	Cinnamomum camphora - Camphor Tree
G	Riverview Parkway	Meadow Way to Cottonwood Avenue		
		Parkway (up to 2')	Chitalpa tashkentensis - Chitalpa	Cercis occidentalis - Western Redbud

Key	Street	Segment	Primary Tree	Secondary Tree
H	Riverview Parkway	Cottonwood Avenue to Magnolia Avenue		
		Median (up to 18')	Metrosideros excelsus - New Zealand Xmas Tree	Pinus canariensis - Canary Island Pine
		Parkway (up to 6')	Ginkgo biloba - Maiden Hair Tree	Alnus glutinosa - Black Alder
		Parkway (up to 7')	Platanus acerifolia - London Plane Tree	Robinia pseudoacacia - Black Locust
I	Edgemoor Drive	Third Street to Riverview Parkway		
		Parkway (up to 2')	Lophostemon confertus - Brisbane Box	Lagerstroemia indica - Crape Myrtle
J	Cottonwood Avenue	Park Center Drive to Riverview Parkway		
			Magnolia grandiflora - Southern Magnolia	Cinnamomum camphora - Camphor Tree
K	Riverwalk Drive	Park Center Drive to Cuyamaca Street		
		Parkway (up to 4')	Quercus suber - Cork Oak	Quercus ilex - Holly Oak
L	Park Avenue	Riverview Parkway to Magnolia Avenue		
		Parkway (up to 4')	Quercus suber - Cork Oak	Quercus ilex - Holly Oak

Street Tree Clearances

Street trees shall be planted between a minimum of fifteen (15) feet and a maximum of twenty (30) feet to the satisfaction of the City Engineer.

Double rows of trees are encouraged, with trees triangulated at the distances listed above. Tree spacing should be equal between street lights, where feasible.

Trees can be grouped and do not have to be equally spaced if alternative layouts are warranted due to conflicts with driveways, utilities, or other design elements.

- ▶ Fifteen (15) feet minimum from street lights.
- ▶ Five (5) feet minimum from driveway apron flares.

- ▶ Twenty (20) feet minimum from the front of any traffic signal or stop sign.
- ▶ Ten (10) feet minimum from the back of any traffic signal or stop sign.
- ▶ Five (5) feet minimum from underground utilities.
- ▶ Ten (10) feet minimum from sewer lines.

Street trees must be installed with an irrigation system designed by a registered Landscape Architect or irrigation consultant, root control barriers, and where appropriate tree grates shall be used. Use of structural soil allows for the elimination of the root control barrier in most cases, and is encouraged.

Special Condition Clearances

In locations where historic or otherwise protected trees occur, such as west of Magnolia Ave within site 20, the following measures, shown in [Figure 3-23 and 3-24](#), should be taken to ensure the designated trees remain undisturbed and integrated into the proposed development.

Figure 3-23: Park Around Existing Trees - Plan View

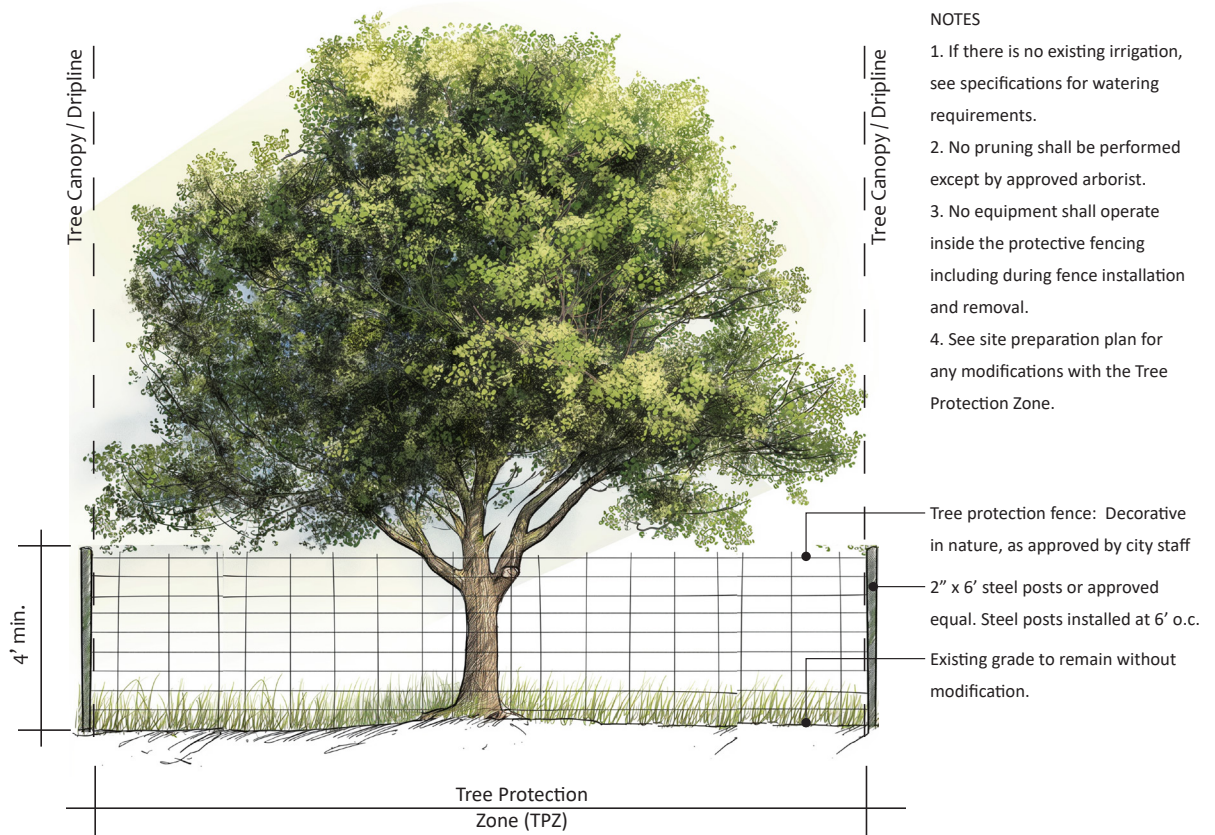


- ① Adjacent Development
- ② New Sidewalk
- ③ Bioswale With Boulders And Crushed Stones
- ④ Berm Planting Area With Boulders
- ⑤ Interactive Rest Area - Around Existing Trees
- ⑥ Existing Trees (Typical)
- ⑦ Proposed Trees (Typical)

Figure 3-24: Park Around Existing Trees - Elevation

WATERING

1. The Contractor shall be fully responsible to ensure that adequate water is provided to the tree (if required) or existing irrigation preserved during the entire construction period. Adequate water is defined to be maintaining soil moisture above the permanent wilt point to a depth of 8 inches or greater.
2. The Contractor shall adjust the automatic irrigation system, if available, and apply additional water, using hoses or water tanks as required.
3. Periodically test the moisture content in the soil within the root zone to determine the water content.



NOTES

1. If there is no existing irrigation, see specifications for watering requirements.
2. No pruning shall be performed except by approved arborist.
3. No equipment shall operate inside the protective fencing including during fence installation and removal.
4. See site preparation plan for any modifications with the Tree Protection Zone.

- Tree protection fence: Decorative in nature, as approved by city staff
- 2" x 6" steel posts or approved equal. Steel posts installed at 6' o.c.
- Existing grade to remain without modification.

At a minimum, the tree protection fence should extend to the dripline. The optimal distance is to the tree's critical root zone. No grading, storage of material, soil compaction, or parking of vehicles is permitted in the TPZ. Existing grade within the tree protection zone is to remain as is.

Landscape in Public Spaces

Landscape is an important part of connecting the Specific Plan Area to the San Diego River. In order to strengthen that connection, even when not directly connected to the river landscape areas within public spaces such as pocket parks, near intersections, along special edge conditions such as the Art Trail are important features and should be integrated into the site development process. The following figures (3-25, 3-26, and 3-27) show examples of how landscape should be used as a tool for placemaking as well as to create a sense of being enveloped in the natural amenities that Santee has to offer.

Figure 3-25: Landscape Improvements In Public Spaces



- ① Adjacent Development
- ② New Sidewalk
- ③ Bioswale Parkway
- ④ Berm planting area
- ⑤ Interactive Rest Area - Park Pocket
- ⑥ Class II Bike Lane

Figure 3-26: Pocket Park Concept



- ① Adjacent Development
- ② New Sidewalk
- ③ Bioswale Parkway
- ④ Berm planting area
- ⑤ Interactive Rest Area - Park Pocket
- ⑥ Landscape Buffer Between Development and Federal Institution
- ⑦ Storm Water Treatment Planter
- ⑧ Landscape Buffer - Riparian Area

Figure 3-27: Public Space Along Special Edge Conditions



- ① Boardwalk Bridge
- ② Landscape Buffer - Riparian Area
- ③ Grass Entertainment Area
- ④ Interactive Rest Area
- ⑤ Landscape Buffer Between Development and Federal Institution
- ⑥ Native Planting Area

3.5.2 Paving

Paving Treatments

The sidewalk treatment in the identified locations shall relate to the character, use and landscape of each neighborhood. Please refer to [Figure 3-28: Paving Plan](#) and [Figure 3-29: Standard Paving Treatments](#).

Figure 3-28: Paving Plan

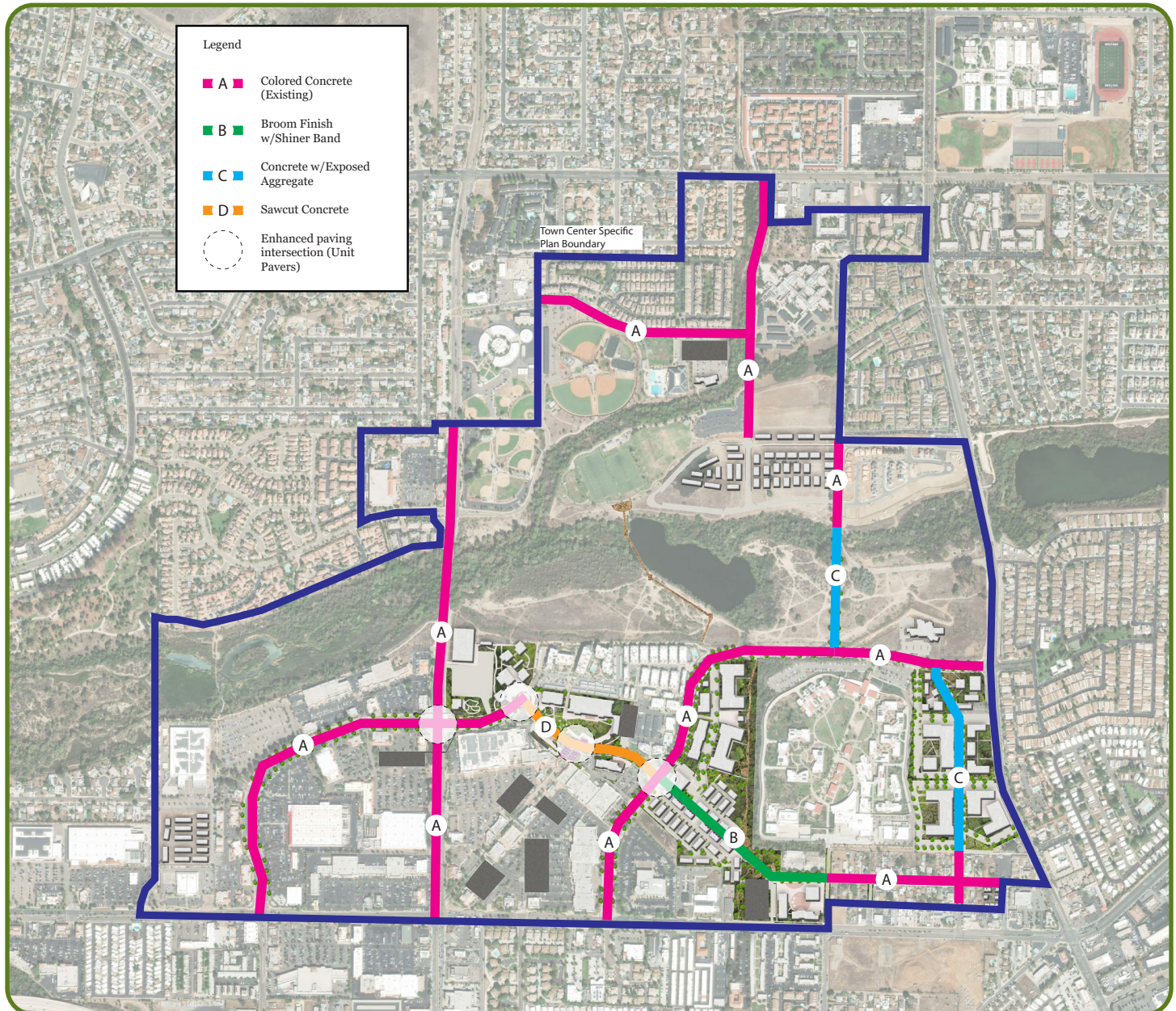


Figure 3-29: Standard Paving Treatments

If using unit pavers, they must be standard shape, size, and colors that are readily available off the shelf.

Sidewalk Width

The minimum sidewalk width shall be 5 and 1/2 foot, including curb. Sidewalk widths shall accommodate pedestrian walking space, and street furniture in compliance with the Americans with Disabilities Act (ADA) and Title 24 California Administrative Code. Given the existing development pattern in the Arts and Entertainment Neighborhood, additional right-of-way dedication to widen sidewalks is not recommended.

Street Corner Standard

Standard street corners shall have a minimum radius of thirty (30) feet. Except for Cuyamaca Street and Town Center Parkway, where a minimum radius of forty (40) feet shall apply. The curb radius may be reduced, upon approval by the City Engineer, where warranted by existing conditions or conflicts.

Parking should be prohibited within ten (10) feet from the point of return of the curb radius to allow for appropriate sightlines.

Street Corner Pop-outs

Street corner pop-outs as identified shall emphasize pedestrian activity, mitigate traffic impacts, and enhance the streetscape. They should be used on streets and in areas where shown on the conceptual plans in Chapter 2. Street corner pop-outs shall meet the twenty (20) foot curb radius standard and support drainage and street sweeping equipment.

Corner Clear Zone

A clear zone shall be reserved at all street corners to permit unobstructed pedestrian movement and increase visibility. The clear zone shall be the area

created by extending a line from each respective property line at the corner to the curb.

Within this zone, all lights, traffic boxes, mail boxes, newspaper machines, transformers, litter containers, landscaping and planters, and pedestrian information signs are prohibited.

The clear zone shall be kept free of traffic signals where feasible.

Litter Receptacles

Litter receptacles in the right of way are required for new developments with more than one hundred (100) linear feet or more of street frontage. The frontage of each development shall have at least two (2) litter receptacles located on each corner.

Removal and Addition of Street Trees

The permit process for removal and addition of street trees within the Town Center Specific Plan area shall follow City guidelines as established by the Community Services Department.

Tree Grates

Tree grates shall meet safety requirements of ADA and Title 24 of the State Administrative Code. The tree grate shall be installed flush with adjacent paving in a one-piece steel frame unit embedded in concrete and be anti-pilfer equipped (two-piece steel frame units will be allowed when installed around existing trees). The standard size tree grates shall be four (4) feet by six (6) feet .

Edges of tree grates should be aligned with score lines and/or expansion joints and adjacent to the concrete curb and gutter. Metal grates shall be factory primed and painted black.

3.5.3 Gateways and Public Signs

As part of creating a cohesive thread through the Specific Plan area, a sign program will be implemented as a tool to orient visitors to the specific location they are in or guide them to where they are going. New development and improvements in the public right-of-way shall implement the Sign Location Plan (refer to Figure 3-30: Sign Location Plan).

Figure 3-30: Sign Location Plan



LEGEND

- | | |
|--------------------------------|--------------------------|
| BRANDING ON BRIDGE | PEDESTRIAN DIRECTIONALS |
| MEDIAN ID MONUMENT | PEDESTRIAN DIRECTORY |
| SUBNEIGHBORHOOD IDENTIFICATION | TRAIL MASTHEAD |
| FACILITY/ZONE IDENTIFICATION | TRAIL INTERPRETIVE SIGNS |
| VEHICULAR DIRECTIONALS | DRIVE-IN SIGN |

Family of Signs

A goal of the sign program is to provide a family of signs that use a common set of materials, colors and style implemented in different ways to accommodate various needs throughout the Specific Plan area.

Figure 3-31 shows a complete concept family which includes the following:

- ▶ A new marquee branding opportunity to be located on the pedestrian bridge that crosses Cuyamaca near the Sportsplex.
- ▶ Median Identification Monument that will identify entries into the Specific Plan Area.
- ▶ Sub-neighborhood identification which will be used to identify each of the various neighborhoods.
- ▶ Facility/zone identification used to identify key buildings or locations throughout the Specific Plan area.
- ▶ Vehicular directionals located at intersections to guide drivers to nearby parking garages, amenities and neighborhoods.
- ▶ Pedestrian directionals located within developments to guide visitors to adjacent neighborhoods or amenities.
- ▶ Pedestrian directories that show a map of the immediate area and important relevant information.
- ▶ Trail mastheads that provide important trail information and maps.
- ▶ Trail interpretive signs that provide information about local wildlife and ecology.

The family of signs are further identified by the neighborhoods they are located in. Each neighborhood has a unique design and color on the back of their sign panels as well as a unique material at their base that relates to the character of each neighborhood, which are shown on Figures 3-32 through 3-40. Signs in the public right of way will reflect the branding and color palette established on Figure 3-41.

In addition to the family of signs, the Arts and Entertainment Neighborhood will incorporate the sign from the Santee Drive-In at a location to be determined within the Entertainment Commercial designated site.

Roof-mounted signs, theater marquee signs and architecturally integrated digital signs shall be permitted within the Arts & Entertainment Neighborhood, and their location, design, size and number shall be determined through a Comprehensive Sign Program. Theater signage may utilize dynamic lighting and static and/or changeable copy signs to promote movies or special events. Roof-mounted signs shall be architecturally integrated and proportional to the building to which it is affixed and must consist of a unique artistic and iconic design.

Figure 3-32: Town Center - Family of Signs

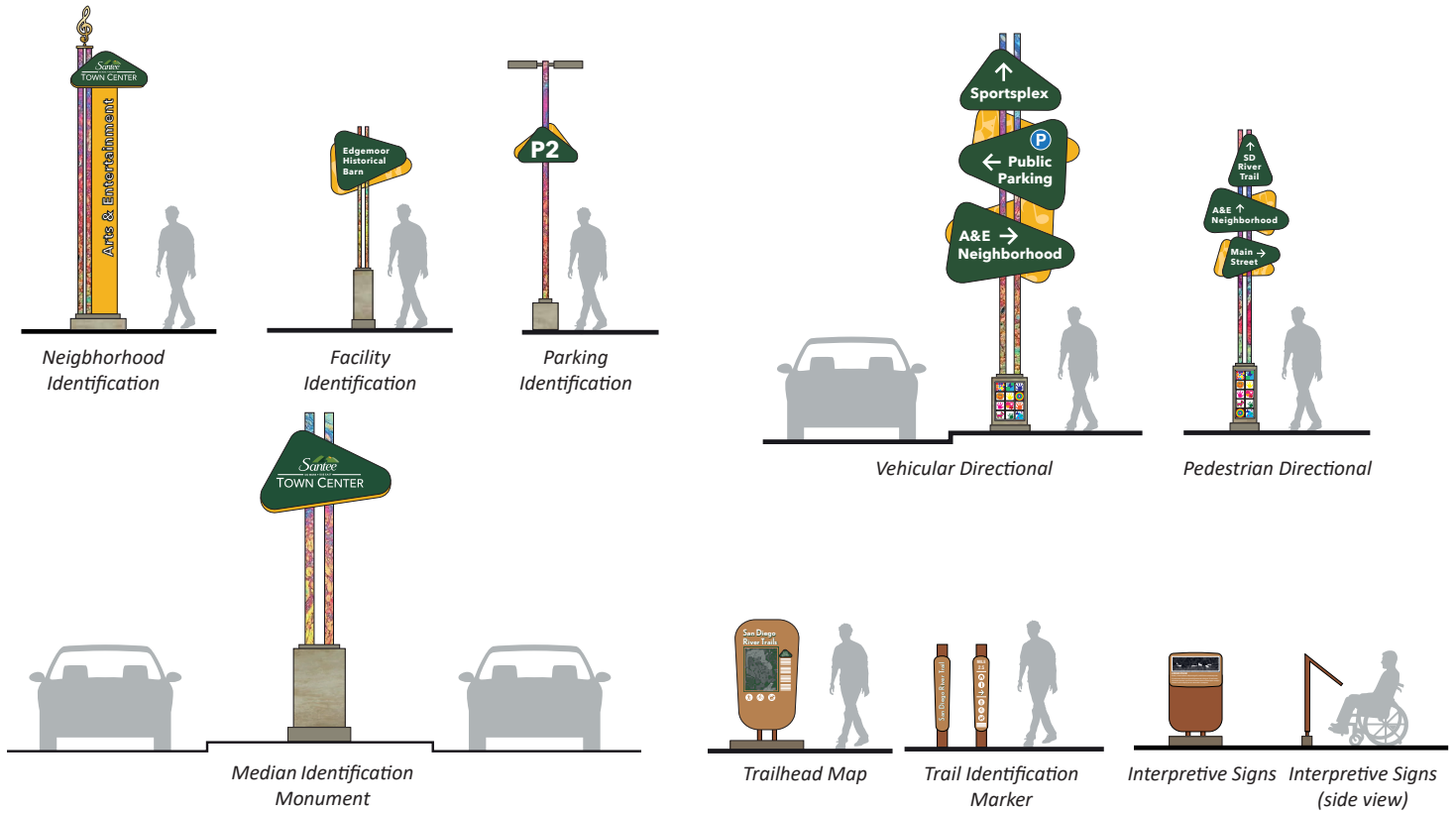


Figure 3-31: Standard Sign Treatment by Neighborhood

Vehicular Directional Signs

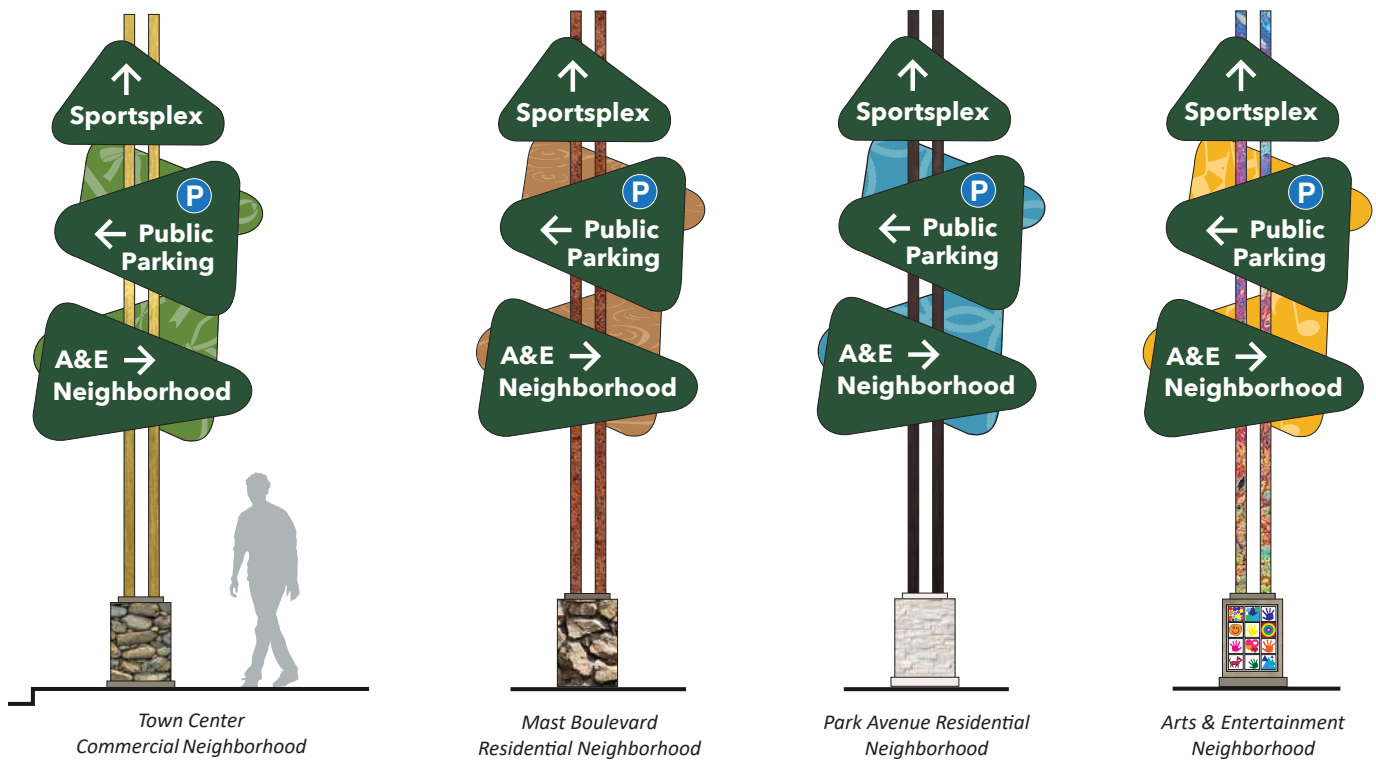


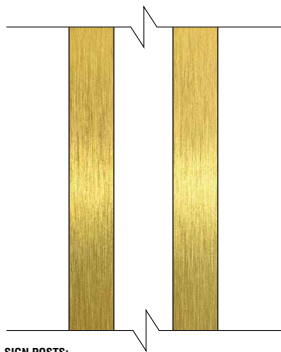
Figure 3-33: Standard Sign Treatment - Town Center Commercial Neighborhood



SIGN PANELS FACES:
DARK GREEN BACKGROUNDS WITH WHITE REFLECTIVE TYPE
(TYPICAL FOR ALL SIGNS)



SIGN PANELS BACK:
SILHOUETTES OF BOWS OR RIBBONS, BOXES WITH BOWS, ETC.
IN GREEN TONE-ON-TONE



SIGN POSTS:
GOLDEN BRASS



SIGN BASE:
COBBLESTONE BASE



EXAMPLE SIGN
SCALE: 3/8" = 1'-0"

Figure 3-34: Standard Sign Treatment - Town Center Residential Neighborhood

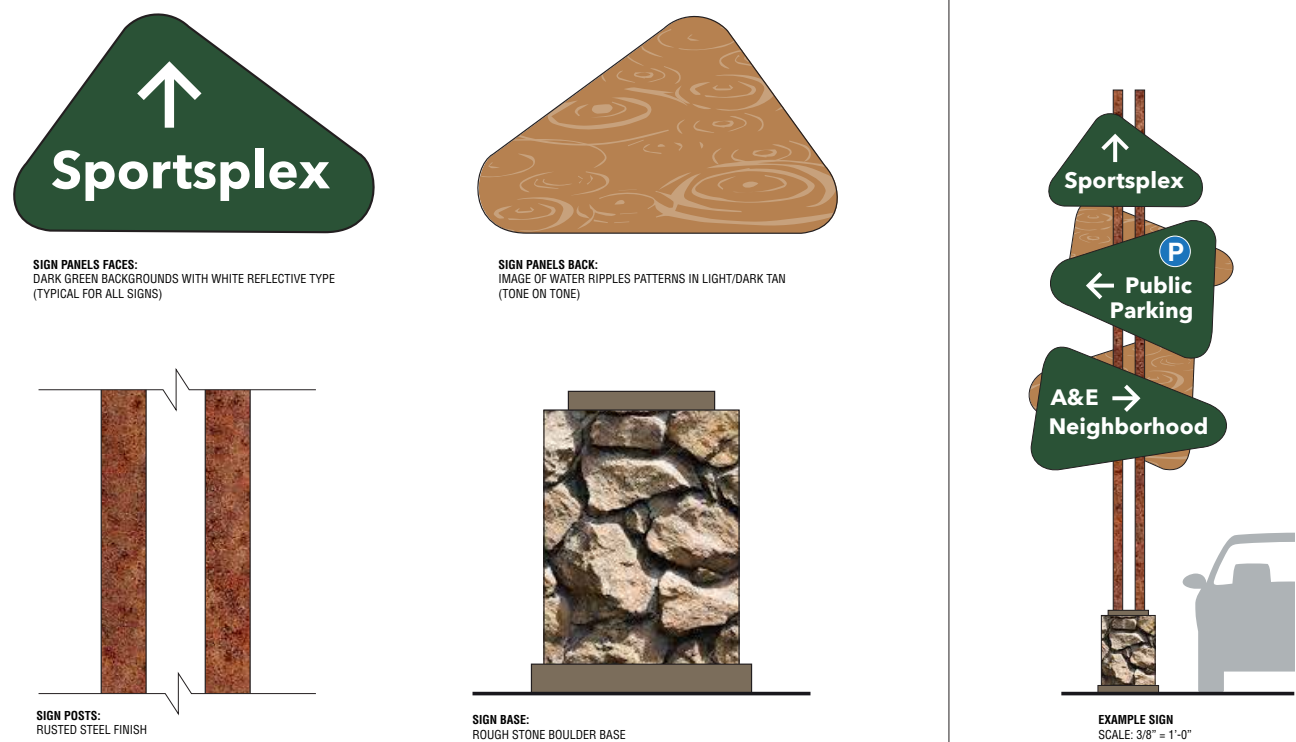
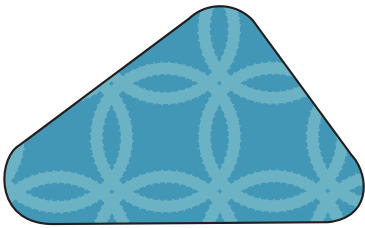


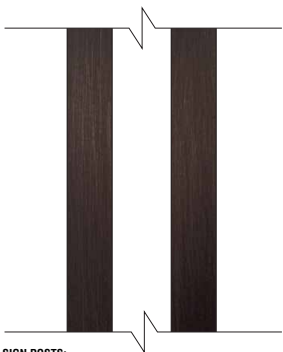
Figure 3-35: Standard Sign Treatment - Park Boulevard Residential Neighborhood



SIGN PANELS FACES:
DARK GREEN BACKGROUNDS WITH WHITE REFLECTIVE TYPE
(TYPICAL FOR ALL SIGNS)



SIGN PANELS BACK:
QUILT PATTERNS IN BLUE TONE-ON-TONE



SIGN POSTS:
RUBBED BRONZE



SIGN BASE:
STACKED LEDGE STONE WITH SQUARE CUT EDGES



EXAMPLE SIGN
SCALE: 3/8" = 1'-0"

Figure 3-36: Standard Sign Treatment - Arts & Entertainment Neighborhood



Figure 3-37: Bridge Treatment - Daytime View



Figure 3-38: Bridge Treatment - Nighttime View



Figure 3-39: Median Identification Monument - Mission Gorge Road and Cuyamaca Street



Figure 3-40: Median Identification Monument - Mission Gorge Road and Riverview Parkway



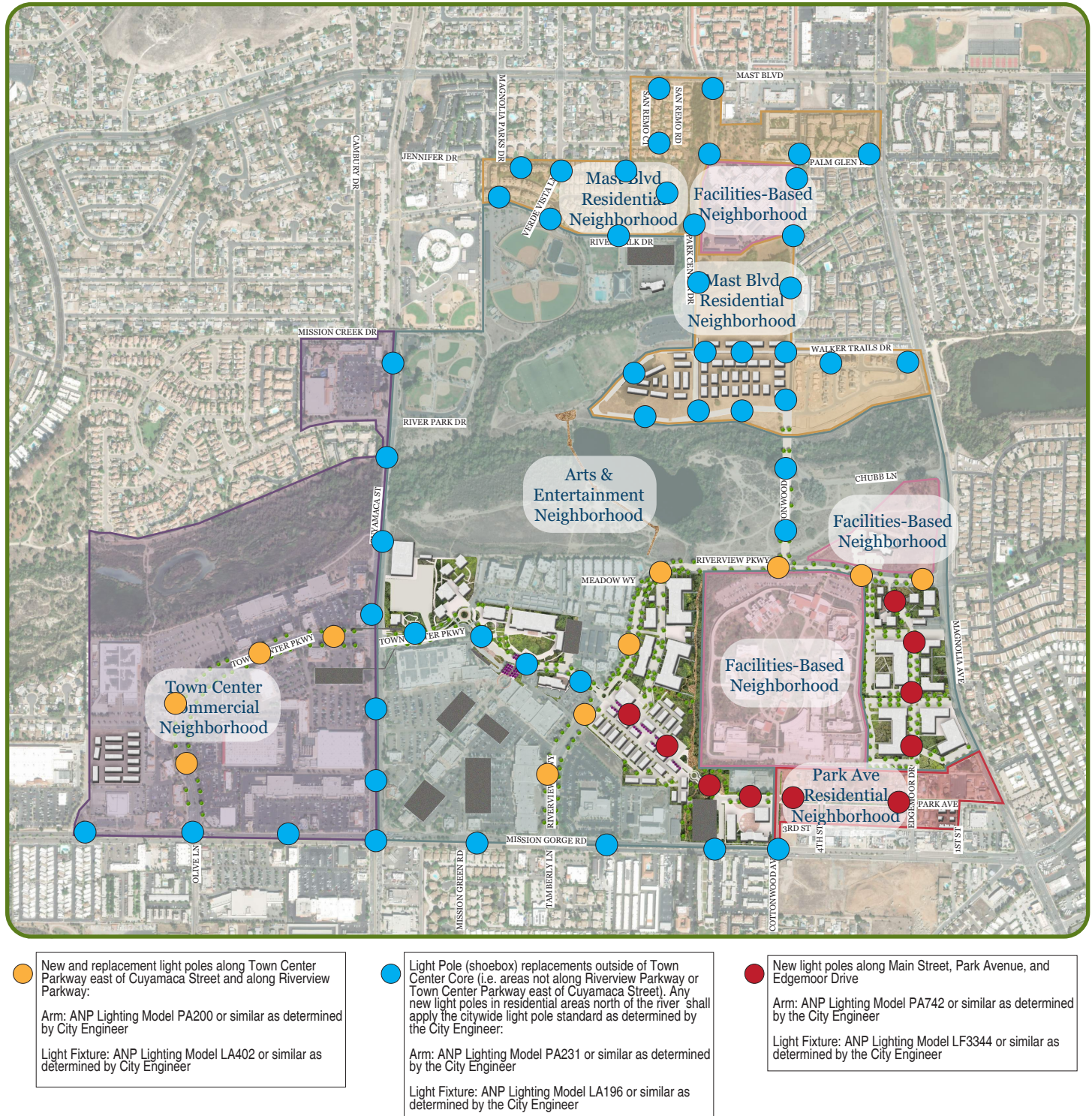
Figure 3-41: Branding Concepts



3.5.4 Street Lighting

Street lighting will be provided in order to adequately light roadways in the Specific Plan area. In order to ensure a cohesiveness between the various neighborhoods specific poles and light fixtures have been selected for implementation and are included in the following pages (refer to [Figure 3-42](#) for Lighting Location). In the event that the specific model of pole or fixture is no longer in production, a pole or fixture similar in aesthetic quality shall be used with the approval of the Planning Director.

Figure 3-42: Lighting Location Plan



Street Lights

Street lights should be located approximately seventy-five (75) to ninety-five (95) feet apart; with a total of three lights located on each 200-foot block frontage and four lights located on each 300-foot block frontage.

Street lights should be triangulated on opposite sides of the street and installed two and a half (2½) feet from the back of curb, measured from the back of curb to center of the pole base.

- ▶ New and replacement light poles along Town Center Parkway east of Cuyamaca Street and along Riverview Parkway shall consist of the following:
 - ▷ Arm ANP Lighting Model PA200 or similar as determined by the City Engineer.
 - ▷ Combined with light fixture APN Lighting Model LA402 or similar as determined by the City Engineer.



ANP Lighting Model PA200 (Arm)



APN Lighting Model LA402 (Light fixture)

- ▶ Light pole (shoebox) replacements outside of the Town Center core (i.e. areas not along Riverview Parkway or Town Center Parkway east of Cuyamaca Street) shall consist of the following. Any new light poles in the Town Center in residential areas north of the river shall apply the citywide light pole standard as determined by the City Engineer.
 - ▷ Arm ANP Lighting Model PA231 or similar as determined by the City Engineer.
 - ▷ Combined with light fixture APN Lighting Model LA196 or similar as determined by the City Engineer.



ANP Lighting Model PA231 (Arm)



APN Lighting Model LA196 (Light fixture)

- ▶ New light poles along Main Street, Park Avenue, and Edgemoor Drive shall consist of the following:
 - ▷ Dual arm ANP Lighting Model PA742 or similar as determined by the City Engineer.
 - ▷ Combined with light fixture APN Lighting Model LF3344 or similar as determined by the City Engineer.



ANP Lighting Model PA742 (Dual arm)



APN Lighting Model LF3344 (Light fixture)

Figure 3-43: Main Street



Figure 3-44: Edgemoor Drive



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